

**CITY COUNCIL AGENDA ITEM**  
CITY OF SHORELINE, WASHINGTON

<b>AGENDA TITLE:</b>	Authorize the City Manager to Execute an Agreement with Sound Transit for the Ridgecrest Park Retaining Wall Betterment as Part of the Lynnwood Link Extension Project
<b>DEPARTMENT:</b>	Public Works City Manager's Office
<b>PRESENTED BY:</b>	Tricia Juhnke, City Engineer Juniper Nammi, Light Rail Project Manager
<b>ACTION:</b>	<input type="checkbox"/> Ordinance <input type="checkbox"/> Resolution <input checked="" type="checkbox"/> Motion <input type="checkbox"/> Discussion <input type="checkbox"/> Public Hearing

**PROBLEM/ISSUE STATEMENT:**

The "Trail Along the Rail" (Trail) is included in the City's approved 2021-2026 Capital Improvement Plan. The Trail project is intended to provide a shared use path aligned roughly parallel to Sound Transit's Lynnwood Link Extension light rail project (LLE Project) between NE 145<sup>th</sup> Street and the NE 195<sup>th</sup> Street Pedestrian Bridge.

Sound Transit is currently constructing the LLE Project which includes segments of a shared use path that will eventually become part of the Trail. However, with necessary cost reductions to the LLE Project, construction of path segments through Ridgecrest Park have been removed by Sound Transit.

Since construction of the Trail's segment through Ridgecrest Park would become difficult, if not impossible, to construct after completion of the LLE Project, in mid-2020, City staff began discussing with Sound Transit the opportunity to modify its design and construct a wall adjacent to Ridgecrest Park that would be utilized in the future for construction of the Trail by the City. The proposed Ridgecrest Park Retaining Wall Betterment Agreement (Attachment A) defines the terms and conditions of this modification.

**RESOURCE/FINANCIAL IMPACT:**

The 2021-2026 Capital Improvement Plan (CIP) includes the Trail Along the Rail Project. A summary of the project expenditures and budget for the Ridgecrest Park Retaining Wall project is shown below:

<b>Expenses</b>			
	City Direct Expenses		(\$10,000)
	Design and Cost Estimating		(\$77,563)
	Betterment Agreement (ST)		(\$703,640)
	Construction Costs	(\$590,000)	
	ST Costs (9.3%)	(\$54,640)	
	Contingency	(\$59,000)	
	Permitting		(\$2,400)
<b>Total Expenses</b>			<b>(\$793,603)</b>
<b>Revenue</b>			
	General Fund		\$203,114
	Roads Capital Fund		\$125,973
<b>Total Revenue</b>			<b>\$329,087</b>
<b>Funding Gap</b>			<b>(\$464,516)</b>
<b>Additional Recommended Revenue Sources</b>			
	Street Vacation Fund		\$197,000
	Additional REET		\$267,516
<b>Total Additional Recommended Revenue Sources</b>			<b>\$464,516</b>

The Ridgecrest Park Retaining Wall project expenditures exceed its current budget. The budget was developed prior to completing the design or obtaining costs from Sound Transit's Contractor (SKH Joint Venture). To fully fund this project, staff has identified two additional funding options:

- Street Vacation - the City has previously collected \$197,000 from street vacations that has not been allocated to the project.
- Additional collection of Real Estate Excise Tax (REET) - in 2020 the City collected \$515,000 above the projections included in the CIP.

These two sources exceed the additional \$467,696 needed to complete the Betterment Wall project. Staff recommends using the Street Vacation revenue and to supplement that with \$270,696 of surplus 2020 REET. A budget amendment would be processed at a later date so as to increase the revenue authorization if Council authorizes the City Manager to execute this agreement.

### **RECOMMENDATION**

Staff recommends that the City Council authorize the City Manager to execute the Ridgecrest Park Retaining Wall Betterment Agreement for the Lynnwood Link Extension Light Rail Transit Project with Sound Transit.

Approved By:           City Manager **DT**   City Attorney **JA-T**

## **BACKGROUND**

The City's Trail Along the Rail Project (Trail Project) is a 2.5-mile-long shared-use path that would roughly be aligned parallel to Sound Transit's LLE Project between NE 145<sup>th</sup> Street and the NE 195<sup>th</sup> Street Pedestrian Bridge. The City's Feasibility Study for the Trail identified the preferred alignment. The Trail Project is included in the approved 2021-2026 Capital Improvement Plan (CIP) and is intended to be funded in segments. Currently, limited funding has been allocated to proceed with design. Staff is seeking grant funds to support this project.

Concurrently with the Trail Feasibility Study, Sound Transit was designing the LLE Project which includes a shared-use path that has segments which could eventually become part of the Trail. The LLE Project design at 60% included a segment of shared-use path that extended through Ridgecrest Park between NE 161<sup>st</sup> and NE 163<sup>rd</sup> Streets. Because it was included in the LLE Project design, the Trail Feasibility Study identified this segment as "easy" to complete.

However, in mid-2017, Sound Transit found unexpected budget overruns based on the 60% design cost estimate of their project and engaged in a cost reduction process. Sound Transit's proposed changes to the LLE Project design were presented to Council on July 16, 2018. After these cost reduction design changes, the LLE Project no longer included construction of a path segment through Ridgecrest park. However, it also did not preclude future construction of the Trail by the City. More information can be found in the July 16, 2018, staff report on this topic: [Sound Transit Lynnwood Link Extension Project Update and Discussion of Comments on In-Progress 90% Design Milestone](#).

## **DISCUSSION**

With the permit review and start of LLE Project construction, it became apparent to City staff that the Trail segment adjacent to Ridgecrest Park would be difficult, if not impossible, to construct after the LLE Project is completed. However, once Sound Transit rebuilds the NE 161<sup>st</sup> street end, the grade difference between NE 161<sup>st</sup> Street and NE 163<sup>rd</sup> Street is such that the Trail Project could be built at less than 5% grade (thereby satisfying ADA accessibility requirements) in roughly a straight line if there is a retaining wall to support the required fill.

Since the LLE Project Final design included a noise wall in the Ridgecrest Park area, City staff negotiated with Sound Transit to convert that wall to a retaining wall if the City designed and funded the additional cost and if Sound Transit was willing to have its contractor construct the retaining wall as a 'betterment' to the LLE Project. In 2020, CIP funds for Trail Project-related design were used to design an alternate wall that could provide lateral support to the Trail through Ridgecrest Park.

City staff negotiated the costs to build the retaining wall with Sound Transit so as to be included in Sound Transit's contract with its contractor, SKH Joint Venture. The construction costs (\$590,000) were negotiated directly with SKH Joint Venture and are based on detailed estimates for quantities and resources required for the retaining wall work above the original noise wall costs already included in the contract. In addition to construction costs, Sound Transit seeks an administrative fee of 9.3% of the

construction costs based on its standard administrative support overhead of 6% and estimates of direct costs for the additional work for its design and construction management consultants.

### **ALTERNATIVES ANALYSIS**

An alternative to the Ridgecrest Park Retaining Wall Betterment Agreement would be delaying construction of a retaining wall until Sound Transit has completed its work in the Ridgecrest Park area and constructing a separate support wall. This alternative would require demolition of the noise wall constructed by Sound Transit along with construction of a new retaining wall which would result in significant costs to the City, as construction would be constrained by LLE light rail operation. Another alternative would be to re-route the Trail away from the light rail guideway, but this would not only create a greater Trail footprint within Ridgecrest Park but would result in grades steeper than 5%, rendering this segment of the Trail less accessible and likely used by fewer people.

In reviewing the alternatives, City staff determined that the proposed Betterment Agreement is the most cost-effective alternative to ensure the Trail Project through this segment of Ridgecrest Park.

### **COUNCIL GOAL(S) ADDRESSED**

The proposed Ridgecrest Park Retaining Wall Betterment Agreement with Sound Transit supports the 2019-2021 Council Goal 3 – “Continue preparation for regional mass transit in Shoreline” and more specifically Action Step 6 - “Create non-motorized connections to the light rail stations and provide for multiple transportation options in and between the Station subareas by continuing to coordinate design elements of the Trail Along the Rail.”

### **RESOURCE/FINANCIAL IMPACT**

The 2021-2026 CIP includes the Trail Along the Rail Project. A summary of the project expenditures and budget for the Ridgecrest Park Retaining Wall project is shown below:

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**RECOMMENDATION**

Staff recommends that the City Council authorize the City Manager to execute the Ridgecrest Park Retaining Wall Betterment Agreement for the Lynnwood Link Extension Light Rail Transit Project with Sound Transit.

**ATTACHMENT**

Attachment A – Ridgecrest Park Retaining Wall Betterment Agreement for the Lynnwood Link Extension Project with Sound Transit

**RIDGECREST PARK RETAINING WALL BETTERMENT  
FOR THE LYNNWOOD LINK LIGHT RAIL TRANSIT PROJECT**

**GA 0207-20/City Receiving #9887**

THIS AGREEMENT, effective upon the date of the latest signature, is entered into by and between the CITY OF SHORELINE, a Washington municipal corporation (the “City”) and the CENTRAL PUGET SOUND REGIONAL TRANSIT AUTHORITY (“Sound Transit”), a regional transit authority organized under the laws of the State of Washington, collectively “Parties” and individually “Party.”

**RECITALS**

1. Sound Transit is a governmental entity created pursuant to RCW 81.104 and 81.112 with all powers necessary to implement a high capacity transit system within its boundaries in King, Pierce, and Snohomish counties.
2. The City is a non-charter optional municipal code city organized pursuant to chapter 35A RCW and incorporated under the laws of the State of Washington, with the authority to enact laws and enter into agreements to promote the health, safety and welfare of its citizens and for other lawful purposes.
3. On April 23, 2015, the Sound Transit Board selected and authorized the implementation of the preferred alternative alignment for the Lynnwood Link Light Rail Extension Project (“LLE Project”) in Resolution 2015-05 (“Alignment Resolution”).
4. In coordination with the City, Sound Transit is in the process of designing and constructing the LLE Project, within the City’s boundaries as described in the Alignment Resolution, including without limitation a public rail transit line, stations, other infrastructure, improvements, public transit and passenger amenities, and mitigation measures associated there with.
5. The City is the owner of a municipal park commonly known as Ridgecrest Park (the “Park”), located just east of Interstate-5, of which the western edge is parallel and adjacent to the LLE Project’s future light rail guideway.
6. As part of that the interagency coordination, the City identified an improvement to the designed and permitted noise wall bordering the Park that is not required to execute the LLE Project, but would provide a public benefit and promote an efficient use of public funds if included as part of the LLE Project.
7. The City identified improvement consists of constructing a retaining wall (“Retaining Wall”) along the westernmost ten (10) feet of the Park in support of the City’s future construction of the City’s “Trail Along the Rail” project, a non-motorized, shared-use path, in lieu of the noise wall originally designed and permitted.
8. In accordance with Sound Transit’s Scope Control Policy (Resolution No. R2009-24), the Parties desire to enter into this Agreement to set forth their respective responsibilities, define the funding commitments for the Retaining Wall and project elements, and build effective cooperation between the Parties.

**NOW, THEREFORE**, in consideration of the recitals, terms, conditions, and covenants contained herein, the Parties hereby agree as follows:

## **AGREEMENT**

### **A. SCOPE OF WORK**

Sound Transit and the City have coordinated during the development of the preliminary design of the Retaining Wall. The purpose of this Agreement is to identify and define the Parties' responsibilities with respect to the Retaining Wall scope of work.

The Retaining Wall will be constructed by Sound Transit's contractor ("Contractor") and the scope of work will be added to the LLE Project L200 contract ("LLE Contract") via a change order. The Retaining Wall will support a City-planned non-motorized, shared use pedestrian and bicycle path (the "Trail Along the Rail") along the west side of the Park and eventually connecting from NE 145th Street to both light rail stations in the City and to the NE 195th Street non-motorized bridge crossing Interstate-5. The Retaining Wall and the preceding items are more fully identified in **Exhibit A** ("Retaining Wall Scope of Work").

### **B. CITY'S RIGHTS & RESPONSIBILITIES**

1. **Priority of Work.** The City understands and agrees that Sound Transit will advance the LLE Project as a whole as a superior goal to advancing the Retaining Wall, if there is a conflict in the priority of the work. The Retaining Wall work will not delay the timely completion of the LLE Project, and Sound Transit has the authority to act to maintain the LLE Project schedule.
2. **Design of Retaining Wall.** The City will be responsible for the design of the Retaining Wall in accordance with all applicable federal, state, and local laws, regulations and ordinances; City and Sound Transit design criteria; the LLE Contract; and the minimum standards established in this Agreement.
  - a. **Conflict of Interest.** The City utilized KPFF, Inc. for design of the Retaining Wall. Sound Transit also utilized KPFF, Inc. on other elements of the LLE Project. As construction occurs, both the City's and Sound Transit's use of KPFF, Inc. may result in a conflict of interest. Therefore, the City agrees to cooperate with Sound Transit in regard to KPFF, Inc. services related to the Retaining Wall design.
3. **Permitting.** Subject to Sound Transit's authorization and as soon as reasonably possible after execution of this Agreement, the City will submit the necessary revisions to Site Development Permit DEV19-0328, so as to replace the currently included noise wall in the Park with the Retaining Wall.
4. **Construction Observation.** The City understands and agrees that construction of the Retaining Wall will be managed by Sound Transit. The City has the right, at all reasonable times, to observe construction of the Retaining Wall and, at its own cost and expense, request additional quality verification or testing of the construction work to be performed. City will request access to observe the Retaining Wall construction work, and Sound Transit will not unreasonably deny any such requests.
5. **Funding.**

- a. Subject to the terms of this Agreement, the City agrees to reimburse Sound Transit for the Estimated Total Cost, as defined below, to construct the Retaining Wall. The City acknowledges that it has funding available for this initial estimate, and that it will provide funding for any valid increases to this amount in accordance with this Agreement.
  - b. Notwithstanding any estimated cost, the City agrees to pay all administrative and construction costs related to the Retaining Wall, except as provided herein.
  - c. The City is not obligated to pay for costs resulting from the sole or partial negligent acts or omissions of Sound Transit or its contractors (if the cost increase is due in part the negligent acts or omissions of Sound Transit or its contractors, the City is not obligated to pay for the portion of the cost increases attributable to such negligent acts or omissions).
  - d. The Parties have agreed to the initial estimate as described in **Exhibit B**. The Parties may further update Exhibit B by mutual written agreement to incorporate cost changes as contemplated by this Agreement.
  - e. The Estimated Total Cost for the Retaining Wall will consist of:
    - i. Contractor's Negotiated Price. The Contractor's proposed price schedule for the Retaining Wall, which will be issued as a change order to the LLE Contract.
    - ii. Sound Transit's Administrative Costs. Sound Transit's costs to administer and oversee the construction of the Retaining Wall. Sound Transit's administrative costs will be 9.3% of the Contractor's Negotiated Price and all approved changes.
    - iii. Contingency. The City will allocate a contingency fund equal to 10% of the Contractor's Negotiated Price to be used for payment arising out of changes. The Contingency will be used to pay for any changes to the Retaining Wall which will increase the cost beyond the Contractor's Negotiated Price and approved according to the provisions of Section D(2) below.
  - f. Notwithstanding this section, the City is obligated to reimburse Sound Transit for all administrative and construction costs related to the Retaining Wall except as provided herein.
- 6. Regulatory Authority.** The City acknowledges it has dual roles concerning the LLE Project and Retaining Wall, as both a land use regulator for the LLE Project within city limits and the funder of the Retaining Wall. Nothing in this Agreement will be deemed an alteration, expansion, or restriction the City's regulatory authority nor a predetermination of the compliance of the Retaining Wall or the LLE Project with applicable federal, state, or local laws, codes, and regulations.
- 7. Acceptance.** The City will be invited to attend acceptance walk-through inspections and may participate in creating acceptance walk-through inspections for the Retaining Wall, consistent with the terms of the LLE Contract. Sound Transit acknowledges that the City's acceptance, which will not be unreasonably withheld, is conditioned on (i) Sound Transit scheduling the acceptance walk-through inspection at a time that the City Designated Representative has agreed to be physically present or to send an authorized representative to be physically present on behalf of the City, (ii) all punch-list items being adequately



addressed according to the requirements of the LLE Contract and to the City's reasonable satisfaction and (iii) that any remaining construction activities related to the Retaining Wall are completed and the Contractor is demobilized for any work pertaining to the Retaining Wall.

### **C. SOUND TRANSIT'S RIGHTS & RESPONSIBILITIES**

- 1. Construction of Retaining Wall.** Sound Transit will include the Retaining Wall work in the LLE Contract as a change order to the existing LLE Contract. If no intervening factors arise between execution of this Agreement and issuance of a change order to the LLE Contract that would otherwise make construction of the Retaining Wall inadvisable, Sound Transit will construct the Retaining Wall at the Contractor's Negotiated Price shown in Exhibit B. Sound Transit will be solely responsible for administering all aspects of the LLE Contract and the Retaining Wall construction work.
- 2. Notice of Costs.** Sound Transit will notify the City in writing when the expenditures for the Retaining Wall have reached eighty percent (80%) of Estimated Total Cost and, at any time Sound Transit has reason to believe the costs for the Retaining Wall could exceed the Estimated Total Cost.

### **D. JOINT OBLIGATIONS – CITY AND SOUND TRANSIT**

- 1. Communication and Participation.** As soon as reasonably possible after execution of this Agreement, the Designated Representatives for the City and Sound Transit will develop a communication and participation plan in relationship to the Retaining Wall. At a minimum, the communication plan will provide for the City's participation in meetings and field inspections, review of documents, and inclusion in all correspondence, written or oral, between Sound Transit and its Contractor.
- 2. Changes.**
  - a.** The City will pay Sound Transit's costs incurred for changes related to the Retaining Wall work approved by the City. Changes include, without limitation, any increases in cost or time caused by unknown conditions in the area of the Retaining Wall relative to the LLE Project, or causes not within the control of the Contractor or Sound Transit, or other valid bases for equitable adjustment pursuant to the LLE Contract.
  - b.** Sound Transit will have authority to administer contract changes within the limits of the Contingency included in the Estimated Total Cost in addition to the Contractor's Negotiated Price, and will make reasonable efforts to cause the Contractor to avoid or mitigate impacts that would cause Retaining Wall costs to exceed the agreed Estimated Total Cost.
  - c.** Changes which increase the cost within the Contingency will be divided into three categories:
 

**Level 1:** Changes that are not site related which increase the cost of the Retaining Wall by no more than \$10,000.00 will be considered automatically approved by the City without any further action necessary. Upon receipt of a Level 1 change, Sound Transit will promptly provide the City notice of the change and its estimated cost

and/or schedule impact. Sound Transit will promptly provide an updated price for the Retaining Wall showing the Contractor's Negotiated Price, cost of all approved changes, and the remaining Contingency.

**Level 2:** Changes that are not site related which increase the cost of the Retaining Wall by more than \$10,000.00 must be submitted to the City for prior approval. The City will review Level 2 changes and, within five (5) calendar days of receipt of the change and estimated costs and/or schedule impacts from Sound Transit, provide Sound Transit with its approval or objection. If the City objects to approval of the change, the City will provide written documentation supporting its objection to Sound Transit. After review of all comments on a proposed change, the Parties will determine whether a change materially affects LLE Project requirements, and if so, the Parties will agree as to whether the proposed change is to be approved, modified, or rejected. After approval or modification, Sound Transit will promptly provide the City with updated price for the Retaining Wall showing the Contractor's Negotiated Price, cost of all approved changes, and the remaining Contingency. Change disputes will be subject to the Dispute Resolution process described in Section H.

**Unsuitable Site Conditions:** Sound Transit will notify the City of any changes needed due to unsuitable site conditions. The Parties will coordinate to determine a solution. In order to maintain LLE Contract schedule and minimize delay, time and materials costs will be tracked and considered automatically approved. Sound Transit will promptly provide the City notice of change and its estimated costs and/or schedule impacts. Sound Transit will promptly provide an updated price for the Retaining Wall showing the Contractor's time and materials, and the remaining Contingency.

3. **Claims.** The City will be responsible for all claims and disputes which arise out of the construction of the Retaining Wall and are outside the control of Sound Transit. The City will not be liable for any disputes or claims related to the Retaining Wall which may arise due to the improper or negligent administration of the LLE Contract by Sound Transit or other negligent acts or omissions by Sound Transit. In consultation with the City, Sound Transit will consider and administer all claims and disputes related to the Retaining Wall in the same manner as any claim or dispute on the LLE Project, in accordance with the LLE Contract. The City may direct Sound Transit in administration of all claims and disputes related exclusively with the Retaining Wall. Sound Transit will inform the City of all disputes or claims that could increase the Estimated Total Cost of the Retaining Wall. The City will pay Sound Transit its reasonable defense costs (attorney's fees and consultant's fees, expenses, and costs; alternative dispute resolution and court costs and expenses), and any settlements, judgments, or awards that are attributable to the City ("Actual Costs"), including those that have been settled through the dispute resolution processes set forth in the LLE Contract. Sound Transit will pay that portion of any Actual Costs that are attributable to the acts or omissions of Sound Transit or its contractor. If a dispute or claim relates to both the Retaining Wall and other LLE Contract scopes, the City will be liable for its pro rata share of the Actual Costs. Sound Transit will consult with the

City for all claims and disputes which relate to both the Retaining Wall and other LLE Contract scopes and take all the City input in good faith and in consideration of the Retaining Wall's relative value to the overall claim or dispute.

- 4. Retaining Wall Ownership and Maintenance.** The Retaining Wall will be owned by Sound Transit as a component of the LLE Project. Sound Transit and the City will share in the responsibility for maintenance of the Retaining Wall and provided herein.
- a. **Operations and Maintenance Agreement.** As soon as reasonably possible after execution of this Agreement, the City and Sound Transit will enter into a separate Operations and Maintenance Agreement for facilities associated with the LLE Project, including the Retaining Wall, so as to more specifically identify the Parties' responsibilities for operations and maintenance of shared or interrelated facilities, such as repairs, replacement, and operationality; the process for track access; equitable allocation of costs; and processes for decision making and dispute resolution. This agreement will be executed no later than Sound Transit's final acceptance of the LLE Project as defined in the LLE Contract.
  - b. In regard to the Retaining Wall, the Operation and Maintenance Agreement will address, at a minimum, the following:
    - i. **City's Maintenance Responsibilities.**  
After acceptance as provided in Section B(7) above, the City will be responsible for periodic inspection and maintenance of the:
 

East side of the Retaining Wall, including graffiti, vegetation management, and other maintenance activities that can be reasonably completed from the east side of the Retaining Wall; Security fencing related to the Retaining Wall; and Footing drains associated with the Retaining Wall.
    - ii. **Sound Transit's Maintenance Responsibilities.**  
After acceptance as provided in Section B(7) above, Sound Transit will be responsible for periodic inspection and maintenance of:
 

West side of the Retaining Wall including graffiti, vegetation management, and other maintenance activities that can be completed from the west side of the Retaining Wall; and Fencing and Drainage Based on its periodic inspections, Sound Transit will promptly bring any issues identified in regard to the fencing or drainage to the City's attention.
    - iii. **Joint Maintenance Responsibilities**  
As part of the periodic inspections, both the City and Sound Transit will inspect the Retaining Wall for structural integrity. If structural repairs and/or replacement is necessary, the Parties will work together in addressing these needs and the associated costs..

## E. PAYMENT

1. **Payment.** The City will pay Sound Transit for all costs incurred by Sound Transit for construction of the Retaining Wall in accordance with this Agreement.
2. **Invoicing.** Sound Transit will invoice the City for fifty percent (50%) of the amount agreed to in Exhibit B within thirty (30) calendar days of the execution of this Agreement. Upon substantial completion of the Retaining Wall, Sound Transit will invoice the City for the remaining fifty percent (50%) plus any additional charges incurred in accordance with Section D(2). The City will pay Sound Transit's invoice within thirty (30) calendar days of receipt of an adequately supported invoice. The City will have access to all supporting documentation for all invoiced amounts. Sound Transit will maintain adequate records for amounts invoiced as provided in Section J below.

#### F. TERM AND TERMINATION

1. **Term.** This Agreement will remain in effect until all required construction as set forth herein is completed and accepted by City; the an easement for the Retaining Wall is recorded, an Operations and Maintenance Agreement is executed, final payment, is made by the City, and all claims related to the Retaining Wall have been resolved.
2. **Termination.** The Parties may terminate all or part of this Agreement by mutual agreement signed by both Parties.
3. **Notice of Default.** Neither Party will be in default under this Agreement unless it has failed to perform a material obligation under this Agreement for a period of thirty (30) calendar days after written notice of default from the non-defaulting Party. A notice of default will specify the nature of the alleged default and the manner in which the default may be cured satisfactorily. If the nature of the alleged default is such that it cannot be reasonably cured within a thirty (30) day period, then the defaulting Party must initiate reasonable actions to cure the default within the thirty (30) day period, and the defaulting Party must thereafter diligently prosecute such cure to completion. If the defaulting Party fails to timely cure the default, the non-defaulting Party may terminate this Agreement.
4. **City Failure to Pay.** The City's failure to timely pay a valid invoice will constitute an immediate default and is not subject to the notice and cure periods described above. Sound Transit will provide written notice of the missed payment and the City will have fifteen (15) calendar days to remit payment plus interest of 12% per annum. If the City fails to remit payment, Sound Transit may terminate this Agreement. If the City disputes a payment amount, it must provide written notice to Sound Transit of the contested invoice amount and the basis of such objection within the thirty (30) calendar days of receipt of the invoice. The Parties will utilize the dispute resolution process in Section H to address this dispute.
5. **Effect of Termination.** If this Agreement is terminated for any reason, the City will reimburse Sound Transit for all costs incurred by Sound Transit due to the inclusion of the Retaining Wall in the LLE Contract, including without limitation, all costs incurred by the subsequent removal of the Retaining Wall from the LLE Contract and any requirements for restoring conditions disturbed during any construction work that may have occurred.

## G. INDEMNIFICATION

- 1. Mutual Indemnification.** Each Party will defend, indemnify and save harmless the other Party, its officers, officials, employees, and agents, while acting within the scope of their employment as such, from any and all costs, claims, judgments, and/or awards of damages (both to persons and property), arising out of, or in any way resulting from, its negligent acts or omissions. Neither Party will be required to defend, indemnify or save harmless the other Party if the claim, suit, or action for injuries, death, or damages (both to persons and property) is caused by the sole negligence of the other Party.
- 2. Concurrent Negligence.** Where such claims, suits, or actions result from concurrent negligence of the Parties, or involve those actions covered by RCW 4.24.115, the indemnity provisions provided herein will be valid and enforceable only to the extent of the Party's own negligence. Each Party agrees that its obligations under this subparagraph extend to any claim, demand, and/or cause of action brought by, or on behalf of, any of its employees or agents. For this purpose, each Party, by mutual negotiation, hereby waives, with respect to the other party only, any immunity that would otherwise be available against such claims under the Industrial Insurance provisions of Title 51 RCW. This indemnification was specifically and mutually negotiated by each of the Parties and will survive the termination of this Agreement.
- 3. Survival.** The indemnification obligations provided in this Section G will survive termination of this Agreement.

## H. DISPUTE RESOLUTION

- 1. Level One.** The Designated Representatives of each Party will use their best efforts to resolve any disputes between the Parties related to or arising out of this Agreement. If an issue is not resolved by informal cooperative efforts, the Designated Representative will notify the other in writing of any issue or dispute they believe requires resolution. Upon receipt of written notification, the Designated Representatives will meet within three days to attempt to resolve the matter. Supporting documentation and information will be provided as requested.
- 2. Level Two.** In the event that a dispute or issue is not resolved by the Designated Representatives, the matter will be referred to the Sound Transit Executive Project Director – Lynnwood Link and the City's Public Works Director, or their designee. The Designated Representatives, individually or jointly, will provide written notice to the Directors that they were unable to resolve the dispute. The Directors will meet within seven (7) business days of the date of the written notice and in good faith attempt to resolve the matter.
- 3. Level Three.** In the event these persons are unable to resolve the matter, the matter will be referred to the Sound Transit Design and Engineering Construction Management Executive Director or Designee and the City of Shoreline City Manager. The Level 2 Directors, individually or jointly, will provide written notice to the Executive Director and City Manager that they were unable to resolve the dispute. The Executive Director and the City Manager will meet and in good faith attempt to resolve the matter within fourteen (14) business days of the date of the written notice.

4. **Exhaustion.** This Dispute Resolution process will be exhausted prior to initiating legal action, but will not be considered the exclusive opportunity or tool to resolve any issues prior to initiating legal action.

#### **I. DESIGNATED REPRESENTATIVES**

The Designated Representatives for each Party, as identified in **Exhibit C**, will be responsible for coordination of any notices or communications between the Parties and will act as the point of contact for each Party for all matters related to this Agreement. The Parties reserve the right to change Designated Representatives by providing written notice to the other Party during the term of this Agreement. Any correspondence or communications related to the Retaining Wall will be made exclusively by and through Sound Transit's Designated Representative or their designee.

#### **J. RECORDS**

1. **Records to be Maintained.** Sound Transit will maintain accounts and records, including contract and financial records, which sufficiently and properly reflect all costs of any nature incurred by Sound Transit and all monies paid by the City to Sound Transit. These records will be maintained for a period of six (6) years after termination or expiration of this Agreement unless permission to destroy the records is granted by the City and the destruction is authorized under RCW Chapter 40.14.
2. **Inspection.** All such Sound Transit's records and documents will be available during regular business hours, upon reasonable request, for inspection, review or audit by the City during the performance of this Agreement and for the required six (6) year period.
3. **Public Records Act.** The Parties are both public agencies subject to Washington's Public Records Act, chapter 42.56 RCW, and that all documents produced in connection with this Agreement may be deemed a public record as defined in the Public Records Act and that if either Party receives a public records request, unless a statute exempts disclosure, the Party must disclose the record to the requestor. Release of a public record pursuant to the Public Records Acts will not be considered a breach of this Agreement nor will the disclosing Party be liable for any cost or expense incurred by the other Party due to disclosure.

#### **K. GENERAL PROVISIONS**

1. **Governing Law and Exclusive Venue.** This Agreement will be interpreted, construed and enforced in accordance with the laws of the State of Washington. The exclusive venue for any action under this Agreement will be King County, Washington.
2. **Successors and Assigns.** This Agreement will be binding upon and inure to the benefit of the successors and assigns of the City and Sound Transit. Neither Party may assign the rights and responsibilities set forth in this Agreement without the express written consent of the other Party.
3. **Time.** Time is of the essence in every provision of this Agreement. Unless otherwise set forth in this Agreement, the reference to "days" will mean calendar days. If any time for action occurs on a weekend or legal holiday, then the time period will be extended automatically to the next business day.

4. **Notice.** All notices or correspondence related to this Agreement will be in writing, addressed to the appropriate Designated Representative. Any notice, request, demand or other communication made pursuant to this Agreement will be deemed received three days after it is mailed, or upon written confirmation by the recipient of receipt by email or by hand delivery.
5. **No Third-Party Beneficiaries.** This Agreement is made and entered into for the sole protection and benefit of the Parties hereto and their successors and assigns. No other person will have any right of action based upon any provision of this Agreement.
6. **No Joint Venture.** No joint venture or partnership is formed as a result of this Agreement. No employees, agents or subcontractors of one party will be deemed, or represent themselves to be, employees of any other party.
7. **Construction.** This Agreement has been reviewed and revised by legal counsel for all parties and no presumption or rule that ambiguity will be construed against the party drafting the document will apply to the interpretation or enforcement of this Agreement. The Parties intend this Agreement to be interpreted to the full extent authorized by applicable law.
8. **Costs.** Each Party will be responsible for its own costs, including legal fees, incurred in negotiating or finalizing this Agreement, unless otherwise agreed in writing by the Parties.
9. **Force Majeure.** Neither party will be liable to the other or deemed in breach or default for any failure or delay in performance under this Agreement during the time and to the extent its performance is prevented by reasons of Force Majeure. For the purposes of this Agreement, Force Majeure means an occurrence that is beyond the reasonable control of and without fault or negligence of the party claiming force majeure and which, by exercise of due diligence of such party, could not have been prevented or overcome. Force Majeure will include natural disasters, including fire, flood, earthquake, windstorm, avalanche, mudslide, and other similar events; acts of war or civil unrest when an emergency has been declared by appropriate governmental officials; acts of civil or military authority; freight embargoes; epidemics; quarantine restrictions; labor strikes; boycotts; terrorist acts; riots; insurrections; explosions; and nuclear accidents. A party claiming suspension or termination of its obligations due to force majeure will give the other party prompt written notice, but no more than five (5) working days after the event, of the impediment and its effect on the ability to perform; failure to provide such notice will preclude recovery under this provision.
10. **Amendments.** This Agreement may be amended only by a written instrument executed by each of the Parties hereto. The Designated Representatives may agree upon amendments to the design for the Retaining Wall and such amendments will be binding upon the Parties without the need for formal approval by the Sound Transit Board and the Shoreline City Council as long as the amendments do not materially alter the functionality or design of the Retaining Wall.
11. **Entire Agreement.** This Agreement constitutes the entire agreement of the Parties with respect to the subject matters of this Agreement, and supersedes any and all prior

negotiations (oral and written), understandings and agreements with respect hereto. However, the Parties will negotiate and execute such ancillary agreements as may be required to implement this Agreement.

12. **Headings.** Section headings are intended as information only and will not be construed with the substance of the section they caption.
13. **Exhibits.** All exhibits attached to this Agreement are hereby incorporated into and made part of this Agreement.
14. **Execution of this Agreement.** This Agreement may be executed electronically on a platform agreed to by the Parties. This Agreement may be executed in several counterparts, each of which will be deemed an original, and all counterparts together will constitute but one and the same instrument.
15. **Severability.** If any of the terms and conditions of this Agreement are determined to be invalid or unenforceable by a court of competent jurisdiction, the remaining terms and conditions thereby will remain in full force and effect.



**CENTRAL PUGET SOUND REGIONAL  
TRANSIT AUTHORITY  
(SOUND TRANSIT)**

**THE CITY OF SHORELINE**

By: \_\_\_\_\_  
Kimberly Farley, Deputy Chief Executive  
Officer

By: \_\_\_\_\_  
Debbie Terry, City Manager

Date: \_\_\_\_\_

Date: \_\_\_\_\_

Authorized by Motion No. \_\_\_\_\_

Approved by City Council Motion on March  
15, 2021

Approved as to form:

Approved as to form:

By: \_\_\_\_\_  
Mattelyn Tharpe, Legal Counsel I

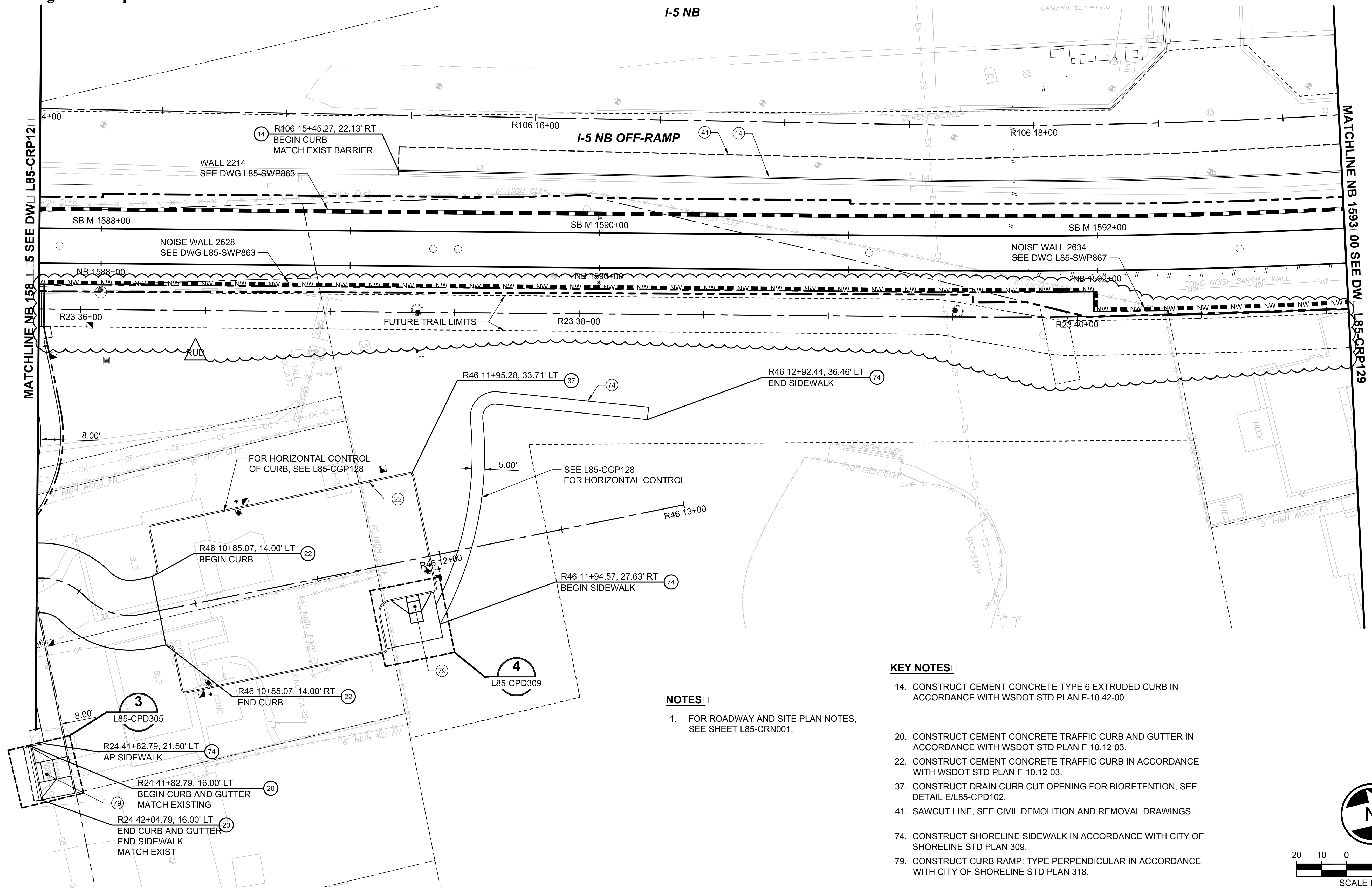
By: \_\_\_\_\_  
City Attorney's Office

**EXHIBITS:**

- Exhibit A: Retaining Wall Scope of Work
- Exhibit B: Retaining Wall Cost Estimate
- Exhibit C: Designated Representatives

**Exhibit A - Retaining Wall Scope of Work**

Xrefs:  
 XL200-G85-GZK020  
 XL200-L85-KAP100  
 XL200-L85-KWP100  
 XL200-L85-SEP100  
 XL200-L85-SZP100  
 XLLE-SHLN-VRX  
 XLLE-SHLN-VBP  
 XLLE-SEAT-VBP  
 GB-SEAL-JQM40855  
 XL200-L85-TSP100  
 XL200-GB-TB22-34  
 XL200-L85-eCAP100  
 XL200-L85-eCAP101  
 XL200-L85-COP100  
 XL200-L85-CLP100  
 XL200-L85-eRPP100  
 XL200-L85-CRP100  
 XL200-L85-UCP100  
 XL200-L85-UCP300  
 XL200-L85-eUCP100  
 XL200-L85-eUCP300  
 XL200-L85-SWP100  
 XL200-L85-CEP  
 XL200-L85-COP200  
 XL200-L85-CAP200  
 XL200-L85-CAP201  
 L200\_CPD SHEET CUTS  
 XL200-L85-JOP100

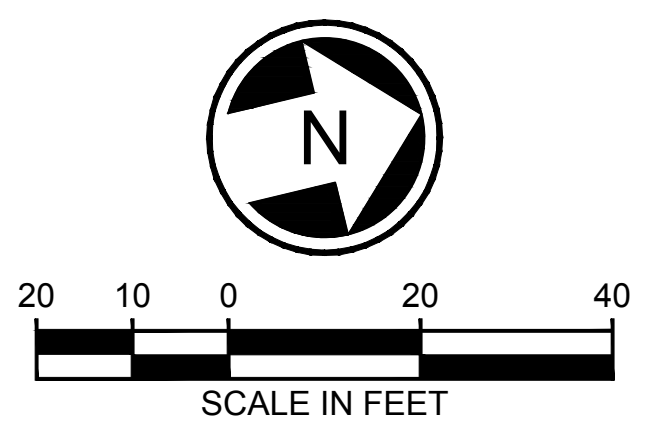


**KEY NOTES**

- 14. CONSTRUCT CEMENT CONCRETE TYPE 6 EXTRUDED CURB IN ACCORDANCE WITH WSDOT STD PLAN F-10.42-00.
- 20. CONSTRUCT CEMENT CONCRETE TRAFFIC CURB AND GUTTER IN ACCORDANCE WITH WSDOT STD PLAN F-10.12-03.
- 22. CONSTRUCT CEMENT CONCRETE TRAFFIC CURB IN ACCORDANCE WITH WSDOT STD PLAN F-10.12-03.
- 37. CONSTRUCT DRAIN CURB CUT OPENING FOR BIORETENTION, SEE DETAIL E/L85-CPD102.
- 41. SAWCUT LINE, SEE CIVIL DEMOLITION AND REMOVAL DRAWINGS.
- 74. CONSTRUCT SHORELINE SIDEWALK IN ACCORDANCE WITH CITY OF SHORELINE STD PLAN 309.
- 79. CONSTRUCT CURB RAMP: TYPE PERPENDICULAR IN ACCORDANCE WITH CITY OF SHORELINE STD PLAN 318.

**NOTES**

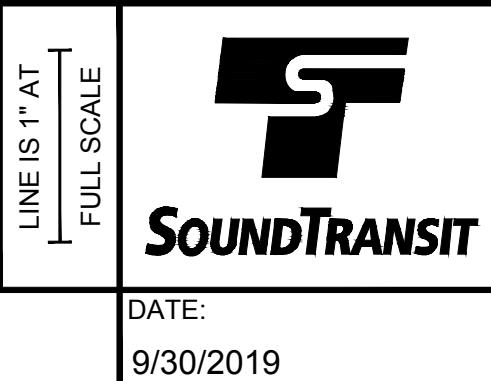
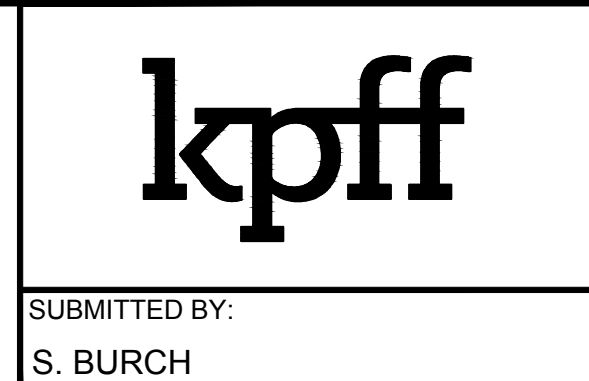
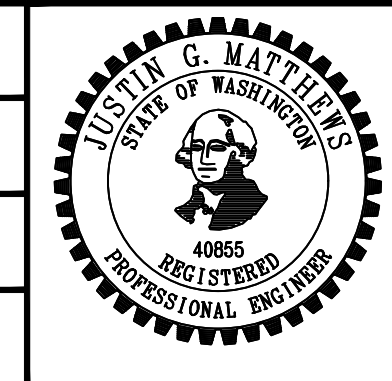
- 1. FOR ROADWAY AND SITE PLAN NOTES, SEE SHEET L85-CRN001.



12/15/20 2:28 PM | SBULL  
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No.	DATE	DSN	CHK	APP	REVISION
RUD	12/07/20	JRG	JGM	JGM	REV UNDER DEVELOPMENT - SHORELINE TAR WALL
A	04/06/20	JRG	JGM	JGM	PR #025 - ZONE 7
0	09/30/19				ISSUED FOR CONTRACT / CO 002

DESIGNED BY:  
**J. GOODMAN**  
 DRAWN BY:  
**T. JOHNSON**  
 CHECKED BY:  
**C. MONKEN**  
 APPROVED BY:  
**J. MATTHEWS**



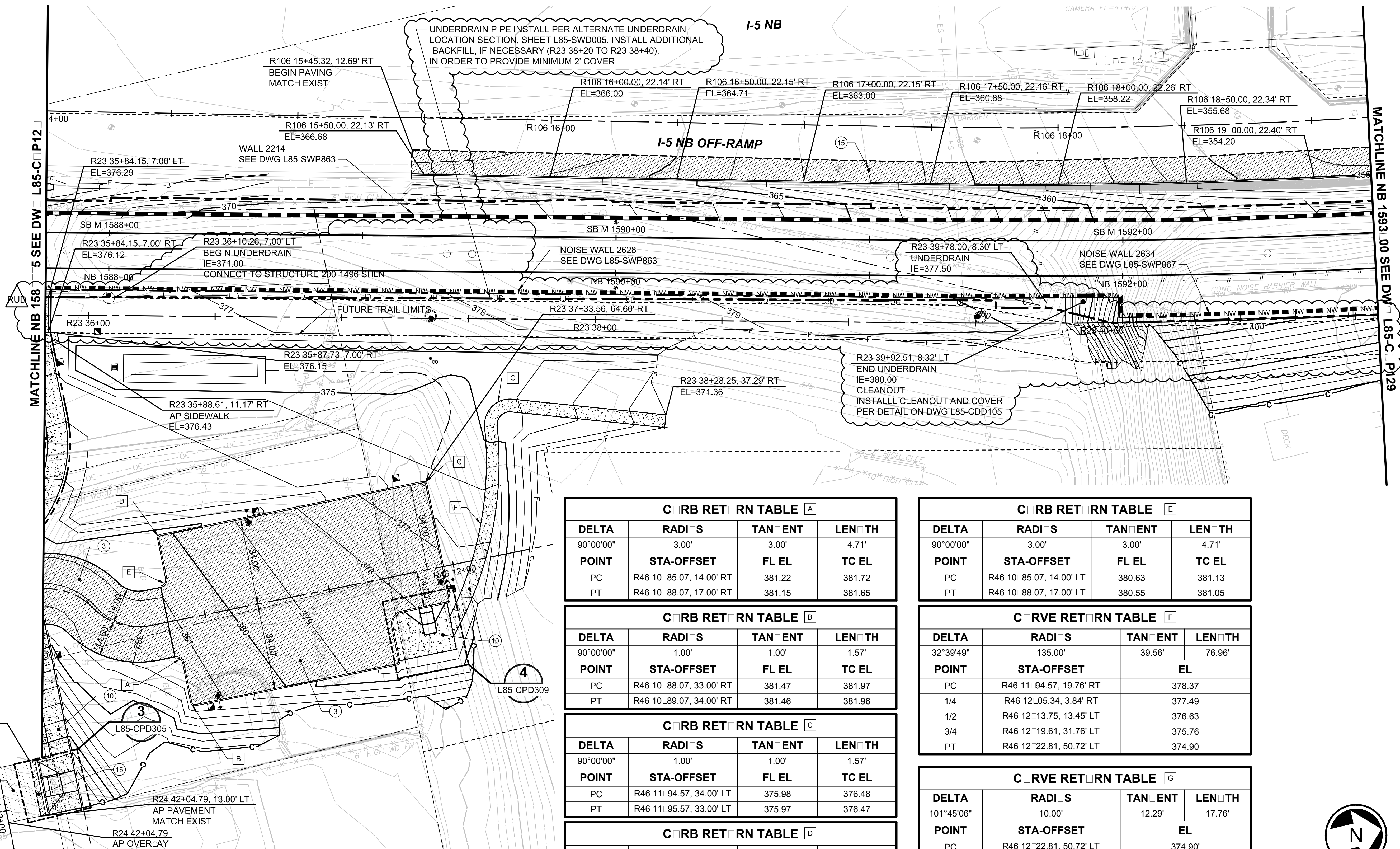
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 CONTRACT No.:  
 RTA / CN 0079-15C  
 DATE:  
 9/30/2019

**LYNNWOOD LINK EXTENSION  
 CONTRACT L200**  
 NORTHGATE STATION TO NE 200TH STREET  
 CIVIL  
 ROADWAY AND SITE PLAN  
 NB 1587+75 TO NB 1593+00

DRAWING No.:	<b>L85-CRP128</b>
LOCATION ID:	<b>N16</b>
SHEET No.:	<b>230</b>
REV:	<b>RUD</b>



Xrefs:  
 XL200-L85-eCAP100  
 XL200-L85-KAP100  
 XL200-L85-SP100  
 XL200-L85-UCP300  
 XL200-L85-UCP100  
 XL200-L85-RPP100  
 XLLE-SHLN-VSP  
 XLLE-SHLN-VRX  
 XL200-L85-CRP100  
 XL200-L85-CDP100  
 XLLE-SHLN-VCN  
 XL200-L85-CGP100  
 XL200-L85-eCAP101  
 XL200-L85-KWP100  
 XL200-L85-SWP100  
 XL200-L85-SZP100  
 GB-SEAL-JOM40855  
 XL200-L85-IB22-34  
 XL200-L85-CP  
 XL200-L85-CLP100  
 XL200-L85-eRPP100  
 XL200-L85-eUCP100  
 XL200-L85-eUCP300  
 XL200-L85-TSP100  
 XL200-L85-CAP200  
 XL200-L85-CAP201  
 XL200-L85-CPD200  
 XL200-L85-CRP100  
 XL200-L85-JOP100



UNDERDRAIN PIPE INSTALL PER ALTERNATE UNDERDRAIN LOCATION SECTION, SHEET L85-SWD005. INSTALL ADDITIONAL BACKFILL, IF NECESSARY (R23 38+20 TO R23 38+40), IN ORDER TO PROVIDE MINIMUM 2' COVER

R23 39+78.00, 8.30' LT UNDERDRAIN IE=377.50  
 R23 39+92.51, 8.32' LT END UNDERDRAIN IE=380.00  
 CLEANOUT  
 INSTALL CLEANOUT AND COVER PER DETAIL ON DWG L85-CDD105

R24 41+82.79, 13.00' LT AP PAVEMENT MATCH EXIST  
 R24 41+82.79 AP OVERLAY  
 R24 42+04.79, 13.00' LT AP PAVEMENT MATCH EXIST  
 R24 42+04.79 AP OVERLAY

**KEY NOTES**

3. PARKING LOT HMA PAVEMENT, SEE DETAIL B/L85-CPD101.
4. HMA PLANE AND OVERLAY, SEE DETAIL H/L85-CPD101.
10. CEMENT CONCRETE SIDEWALK IN ACCORDANCE WITH CITY OF SHORELINE STD PLAN 309.
15. HMA PAVEMENT SECTION, SEE DETAIL A/L85-CPD101.

**NOTES**

1. FOR GRADING AND PAVING PLAN NOTES AND LEGEND, SEE SHEET L85-CGN001.

**CORNER RETURN TABLE A**

DELTA	RADIUS	TANGENT	LENGTH
90°00'00"	3.00'	3.00'	4.71'
POINT	STA-OFFSET	FL EL	TC EL
PC	R46 10+85.07, 14.00' RT	381.22	381.72
PT	R46 10+88.07, 17.00' RT	381.15	381.65

**CORNER RETURN TABLE B**

DELTA	RADIUS	TANGENT	LENGTH
90°00'00"	1.00'	1.00'	1.57'
POINT	STA-OFFSET	FL EL	TC EL
PC	R46 10+88.07, 33.00' RT	381.47	381.97
PT	R46 10+89.07, 34.00' RT	381.46	381.96

**CORNER RETURN TABLE C**

DELTA	RADIUS	TANGENT	LENGTH
90°00'00"	1.00'	1.00'	1.57'
POINT	STA-OFFSET	FL EL	TC EL
PC	R46 11+94.57, 34.00' LT	375.98	376.48
PT	R46 11+95.57, 33.00' LT	375.97	376.47

**CORNER RETURN TABLE D**

DELTA	RADIUS	TANGENT	LENGTH
90°00'00"	1.00'	1.00'	1.57'
POINT	STA-OFFSET	FL EL	TC EL
PC	R46 10+88.07, 33.00' LT	380.24	380.74
PT	R46 10+89.07, 34.00' LT	380.19	380.69

**CORNER RETURN TABLE E**

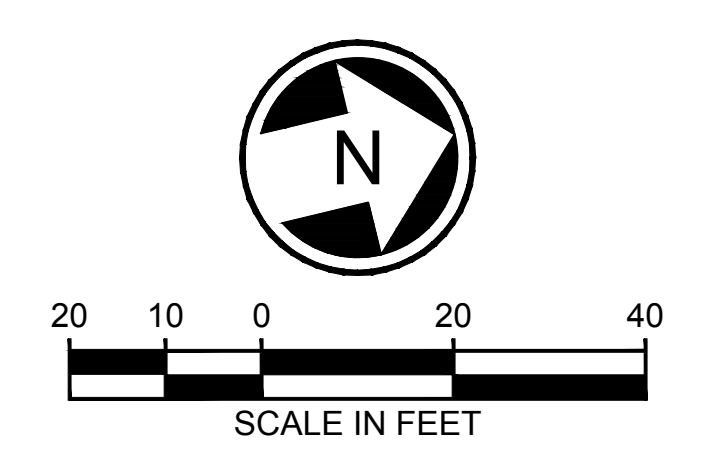
DELTA	RADIUS	TANGENT	LENGTH
90°00'00"	3.00'	3.00'	4.71'
POINT	STA-OFFSET	FL EL	TC EL
PC	R46 10+85.07, 14.00' LT	380.63	381.13
PT	R46 10+88.07, 17.00' LT	380.55	381.05

**CORNER RETURN TABLE F**

DELTA	RADIUS	TANGENT	LENGTH
32°39'49"	135.00'	39.56'	76.96'
POINT	STA-OFFSET	EL	
PC	R46 11+94.57, 19.76' RT	378.37	
1/4	R46 12+05.34, 3.84' RT	377.49	
1/2	R46 12+13.75, 13.45' LT	376.63	
3/4	R46 12+19.61, 31.76' LT	375.76	
PT	R46 12+22.81, 50.72' LT	374.90	

**CORNER RETURN TABLE G**

DELTA	RADIUS	TANGENT	LENGTH
101°45'06"	10.00'	12.29'	17.76'
POINT	STA-OFFSET	EL	
PC	R46 12+22.81, 50.72' LT	374.90'	
1/2	R46 12+27.22, 58.08' LT	374.50'	
PT	R46 12+35.73, 59.31' LT	374.10'	

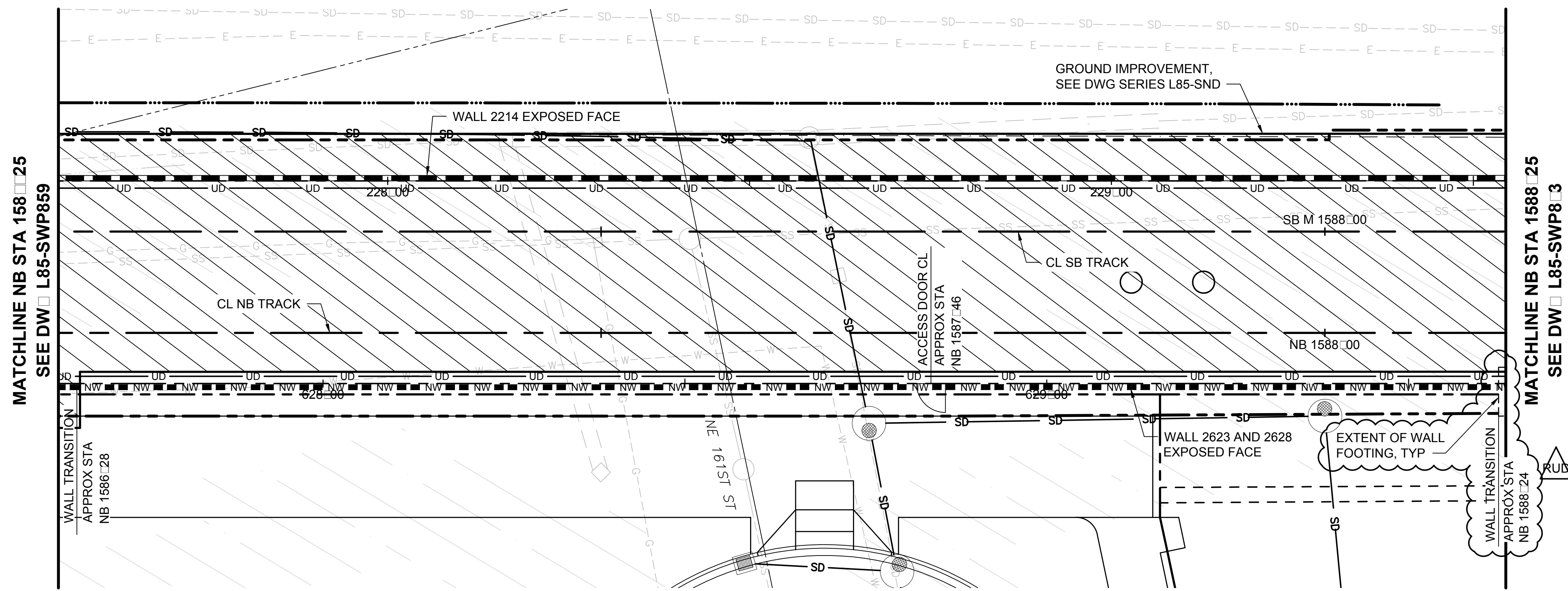


12/15/2019 1:23:23 PM | SBULL  
 Z:\1600001-160099\160216 (LYNNWOOD LINK)CAD\DESIGN\DWG\DOWNLOADS\L200-L85-CGP128.DWG

DESIGNED BY: <b>J. GOODMAN</b> DRAWN BY: <b>T. JOHNSON</b> CHECKED BY: <b>C. MONKEN</b> APPROVED BY: <b>J. MATTHEWS</b>							LINE IS "AT FULL SCALE" 	SCALE: 1" = 20' FILENAME: L200-L85-CGP128 CONTRACT No.: RTA / CN 0079-15C DATE: 9/30/2019	DRAWING No.: <b>L85-C-P128</b> LOCATION ID: N16 SHEET No.: 411 REV: RUD
REV UNDER DEVELOPMENT - SHORELINE TAR WALL PR #025 - ZONE 7 CNWD #043 ISSUED FOR CONTRACT / CO 002								SUBMITTED BY: <b>S. BURCH</b> DATE: 9/30/2019	REVIEWED BY: <b>F. CHIHAB</b> DATE: 9/30/2019

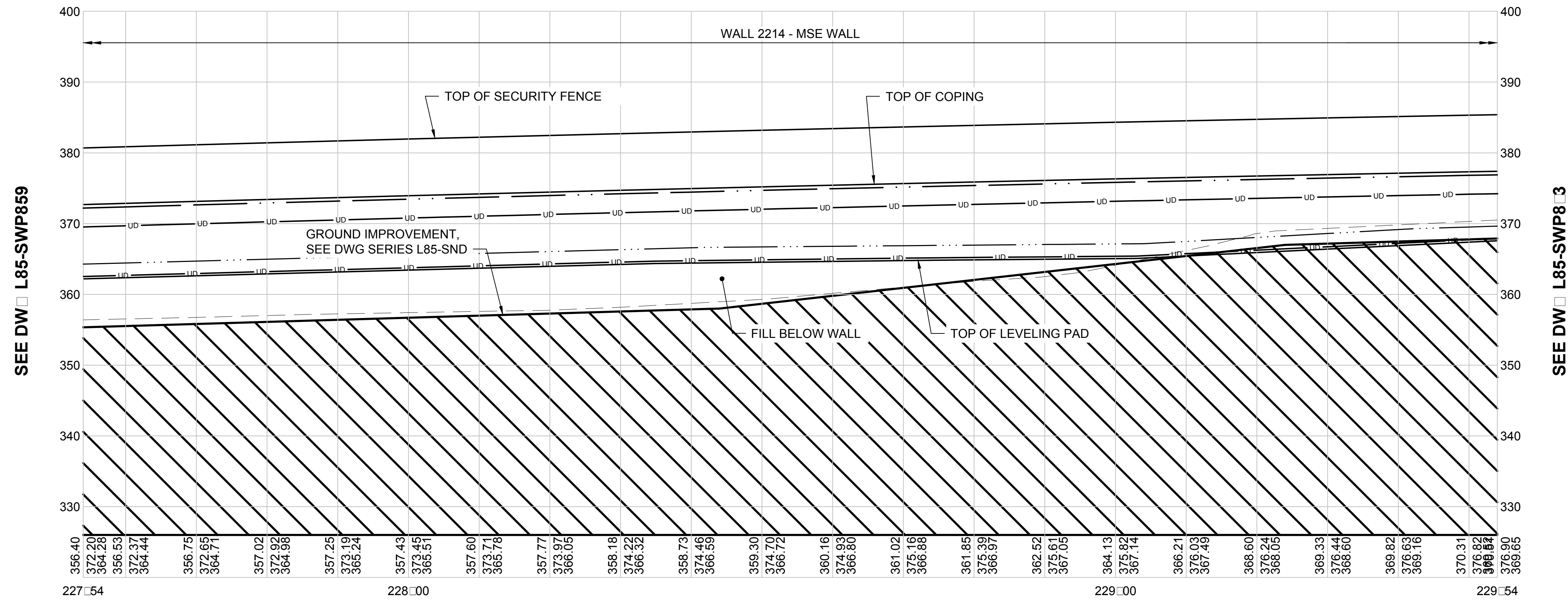


Xrefs:  
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 XL200-L85-CBP100  
 XL200-L85-CBP100  
 XL200-L85-KAP100  
 GB-SEAL-BVE26489  
 XL200-L85-KW100  
 XLLE-SHLN-VBP  
 XLLE-SHLN-VRX  
 XL200-L85-SWP901  
 XL200-L85-SWP301  
 XL200-L85-KWP110  
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 XL200-L85-SNP100  
 XL200-L85-ERP100



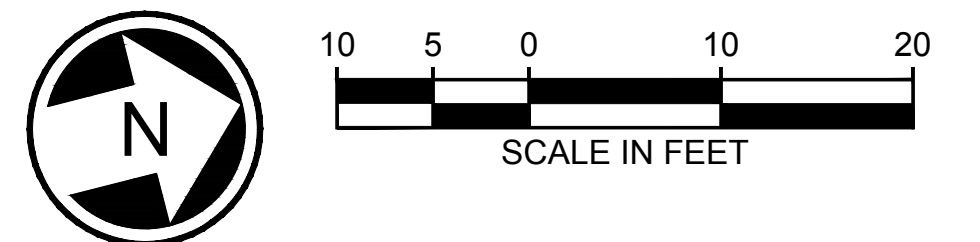
**WALL PLAN - NB 158+25 TO NB 1588+25**

- NOTES**
- SEE DRAWING L85-SWP800 FOR TYPICAL WALL PLAN AND PROFILE NOTES.
  - SEE DRAWING L85-SWP862 FOR WALL GEOMETRY SCHEDULES.



**SB WALL PROFILE - STA 22+54 TO STA 229+54**

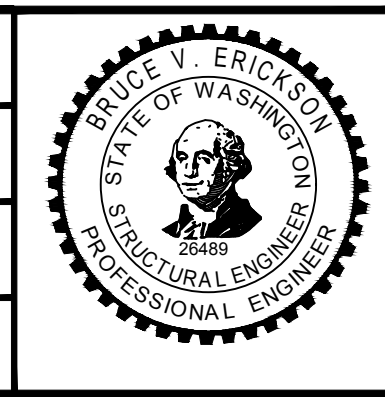
- LEGEND**
- TRACK ALIGNMENT
  - RETAINING WALL ALIGNMENT
  - RETAINING/NOISE WALL ALIGNMENT
  - TOP OF TRACK BALLAST
  - FINISH GRADE OPPOSITE TRACK SIDE OF WALL
  - EXISTING GRADE
  - TRACK AND WALL UNDERDRAINS
  - LOWER FINISHED GRADE ELEVATION WHERE SHOWN
  - UPPER FINISHED GRADE ELEVATION
  - EXISTING GRADE ELEVATION OR FINISHED GRADE ELEVATION OPPOSITE TRACK SIDE



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RUD	12/07/20	JS	BE	BE	REV UNDER DEVELOPMENT - SHORELINE TAR WALL
0	09/30/19				ISSUED FOR CONTRACT / CO 002
No.	DATE	DSN	CHK	APP	REVISION

DESIGNED BY:  
D. EVANGER  
 DRAWN BY:  
H. SHI  
 CHECKED BY:  
B. ERICKSON  
 APPROVED BY:  
B. ERICKSON



**kpff**  
 SUBMITTED BY:  
S. BURCH

**HNTB Jacobs**  
 trusted design partners  
 DATE:  
9/30/2019

LINE IS "AT" FULL SCALE  
 REVIEWED BY:  
F. CHIHAB  
 DATE:  
9/30/2019

**SOUNDTRANSIT**  
 SCALE:  
1" = 10'  
 FILENAME:  
L200-L85-SWP861  
 CONTRACT No.:  
RTA / CN 0079-15C  
 DATE:  
9/30/2019

**LYNNWOOD LINK EXTENSION  
 CONTRACT L200**  
 NORTHGATE STATION TO NE 200TH STREET  
 STRUCTURES, WALLS  
 PLAN, PROFILES & SCHEDULES  
 NB 1586+25 TO NB 1588+25 SHT 1

DRAWING No.:  
**L85-SWP861**  
 LOCATION ID:  
N16  
 SHEET No.:  
1580  
 REV:  
RUD

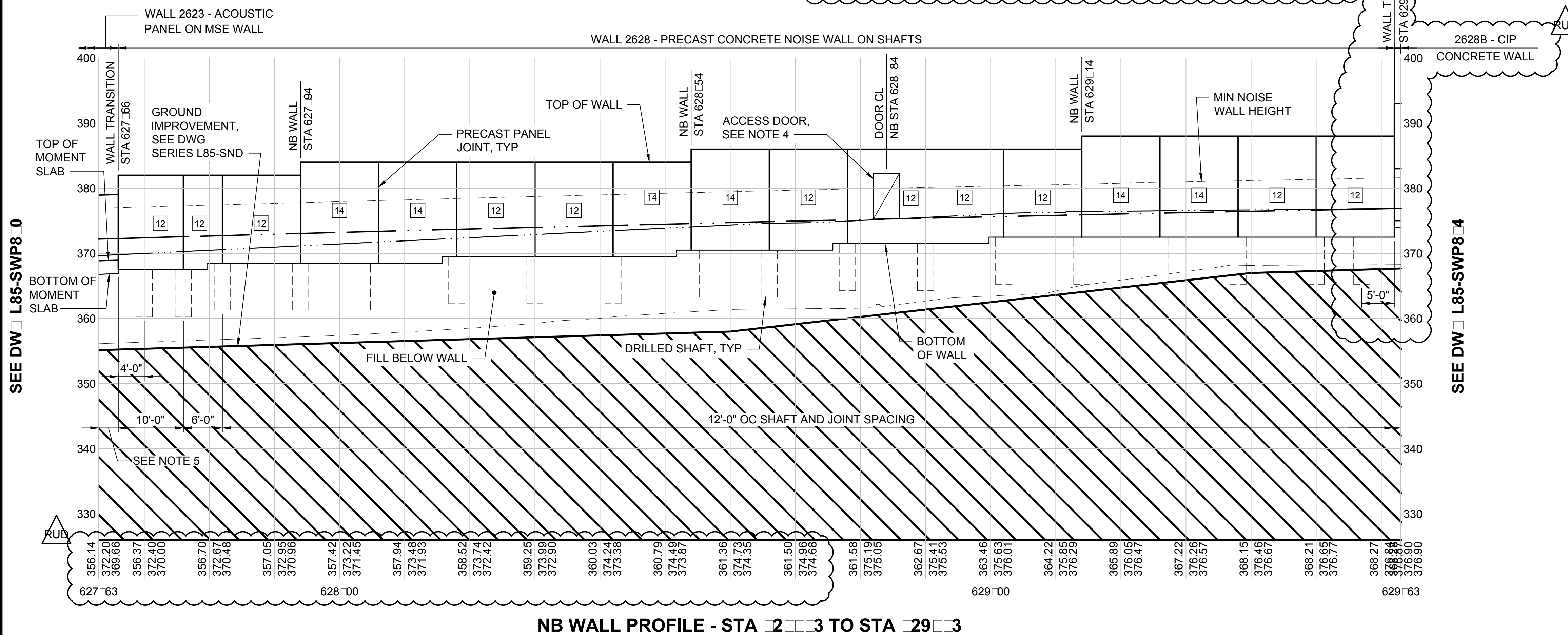


Xrefs:  
 XL200-GB-TB22x34  
 GB-SEAL-SW26489  
 XL200-L85-KW100  
 XL200-L85-SWP901  
 XL200-L85-KWP110  
 XL200-L85-KAP100  
 XL200-L85-KW101  
 XL200-L85-SW301

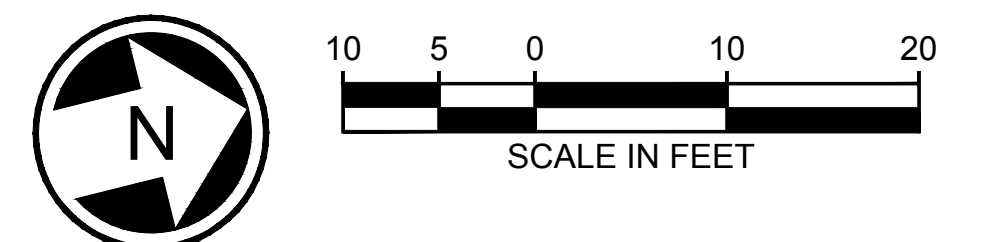
NB WALL SCHEDULE					
HORIZONTAL GEOMETRY					
POINT	WALL STATION	CONTROL LINE NORTH COORDINATE	CONTROL LINE EAST COORDINATE	DATA	OFFSET FROM CONTROL LINE TO WORKLINE
WALL TRANSITION	627+66.00	-	-	-	1'-0 1/2" LEFT 0'-0" RIGHT
VERTICAL GEOMETRY					
WALL STATION	TOP OF WALL ELEVATION	BOTTOM OF WALL ELEVATION			
627+66.00	379.00 ACOUSTIC PANEL 382.00 NOISE WALL	367.50 NOISE WALL			
627+79.75	382.000	367.50 LEFT 368.50 RIGHT			
627+94.00	382.00 LEFT 384.00 RIGHT	368.50			
628+15.75	384.00	368.50 LEFT 369.50 RIGHT			
628+51.75	384.00	369.50 LEFT 370.50 RIGHT			
628+54.00	384.00 LEFT 386.00 RIGHT	370.50			
628+75.75	386.00	370.50 LEFT 371.50 RIGHT			
628+99.75	386.00	371.50 LEFT 372.50 RIGHT			
629+14.00	386.00 LEFT 388.00 RIGHT	372.50			
629+62.00	388.00 LEFT 383.00 RIGHT	372.50 LEFT 375.00 RIGHT			

**NOTES**

- SEE DRAWING L85-SWP800 FOR TYPICAL WALL PLAN AND PROFILE NOTES.
- SEE DRAWING L85-SWP861 FOR WALL PLAN.
- INDICATES EXPOSED WALL HEIGHT, H, PER DRAWING L85-SWD041
- SEE DRAWING L85-SWD110 FOR FOR ACCESS DOOR DETAILS.
- AT CONTRACTOR'S OPTION, OMIT MSE WALL PANELS AT THIS LOCATION AND THICKEN MOMENT SLAB TO ACHIEVE MINIMUM EMBEDMENT AS SHOWN.



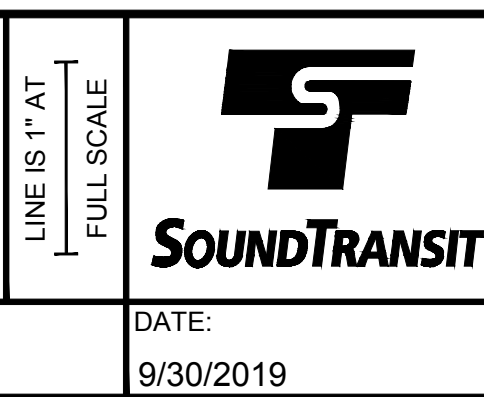
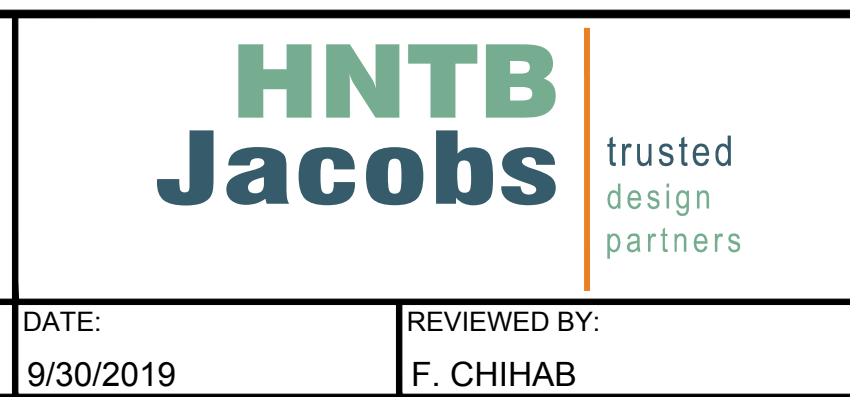
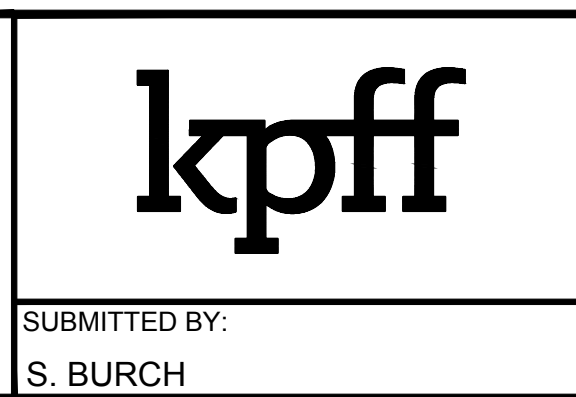
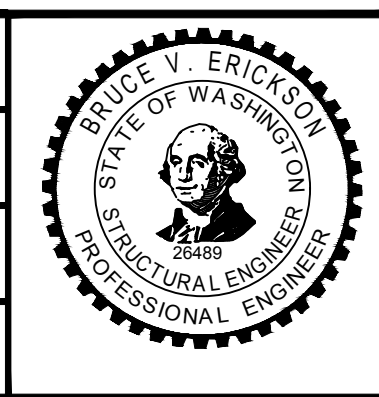
- LEGEND**
- TRACK ALIGNMENT
  - RETAINING WALL ALIGNMENT
  - RETAINING/NOISE WALL ALIGNMENT
  - TOP OF TRACK BALLAST
  - FINISH GRADE OPPOSITE TRACK SIDE OF WALL
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  - TRACK AND WALL UNDERDRAINS
  - LOWER FINISHED GRADE ELEVATION WHERE SHOWN
  - UPPER FINISHED GRADE ELEVATION
  - EXISTING GRADE ELEVATION OR FINISHED GRADE ELEVATION OPPOSITE TRACK SIDE



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No.	DATE	DSN	CHK	APP	REVISION
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A	01/17/20	DE	BE	BE	PR 021
0	09/30/19				ISSUED FOR CONTRACT / CO 002

DESIGNED BY:  
D. EVANGER  
 DRAWN BY:  
H. SHI  
 CHECKED BY:  
B. ERICKSON  
 APPROVED BY:  
B. ERICKSON

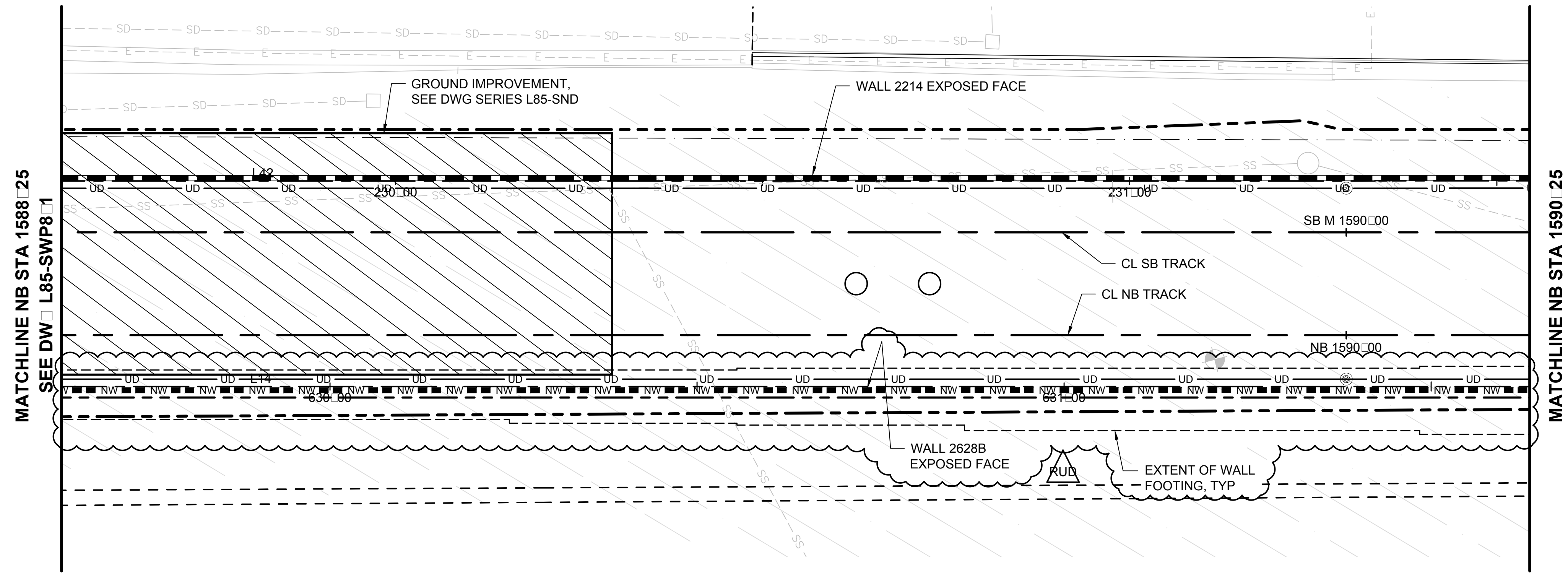


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 CONTRACT No.:  
RTA / CN 0079-15C  
 DATE:  
9/30/2019

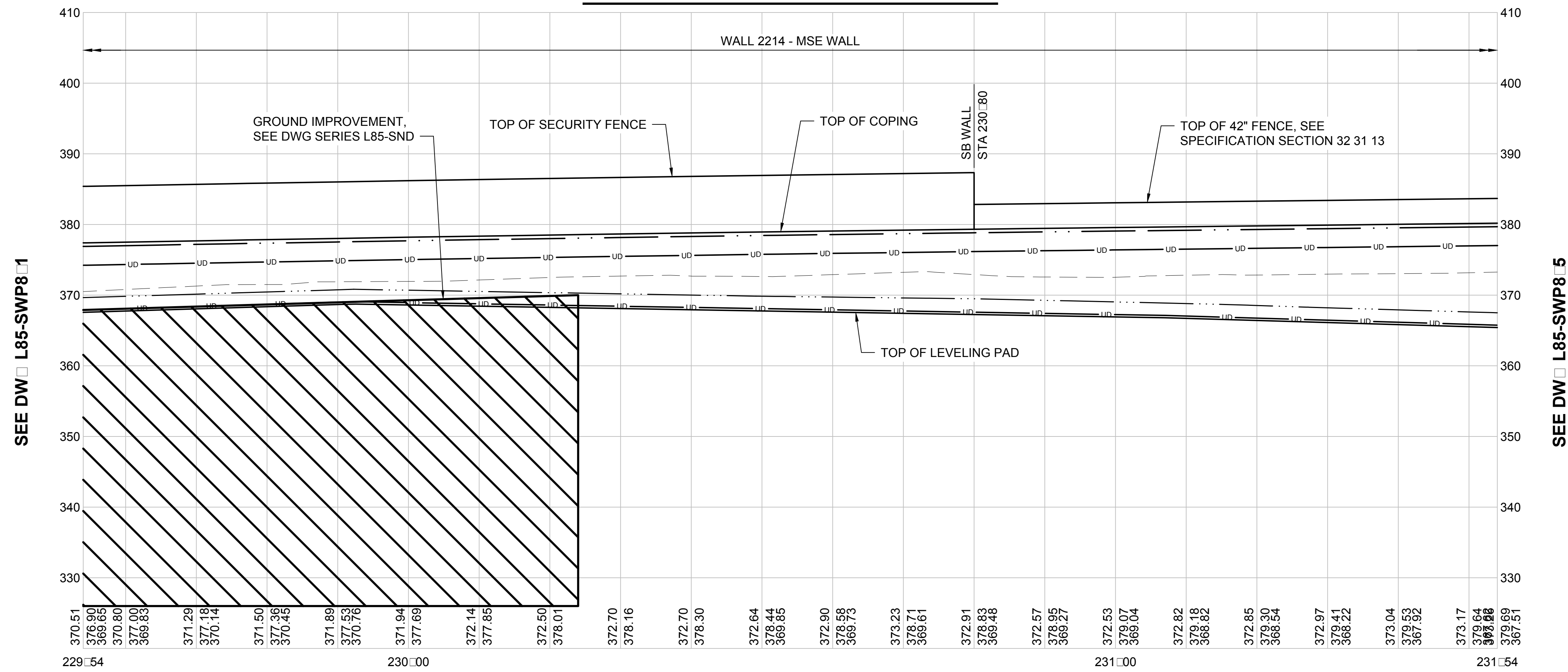
**LYNNWOOD LINK EXTENSION  
 CONTRACT L200**  
 NORTHGATE STATION TO NE 200TH STREET  
 STRUCTURES, WALLS  
 PLAN, PROFILES & SCHEDULES  
 NB 1586+25 TO NB 1588+25 SHT 2

DRAWING No.:  
**L85-SWP802**  
 LOCATION ID:  
N16  
 SHEET No.:  
1581  
 REV:  
RUD

Xrefs:  
 XL200-GB-TB22x34  
 XL200-L85-CDP100  
 XL200-L85-CRP100  
 XL200-L85-KAP100  
 GB-SEAL-BVE26489  
 XL200-L85-KWV100  
 XLLE-SHLN-VSP  
 XLLE-SHLN-VRX  
 XL200-L85-SWP901  
 XL200-L85-SWV301  
 XL200-L85-KWP110  
 XL200-L85-JOP100  
 XL200-L85-KWV101  
 XLLE-SHLN-CP  
 XL200-L85-SNP100  
 XL200-L85-ERP100



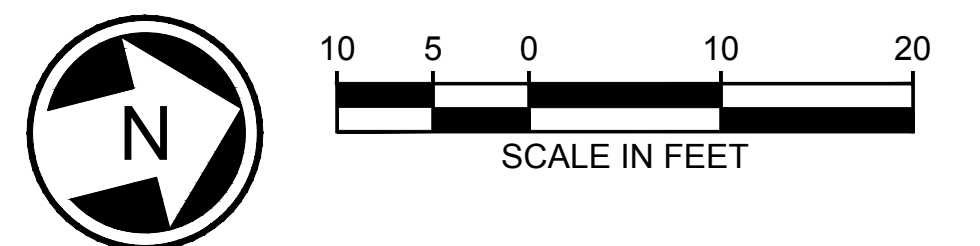
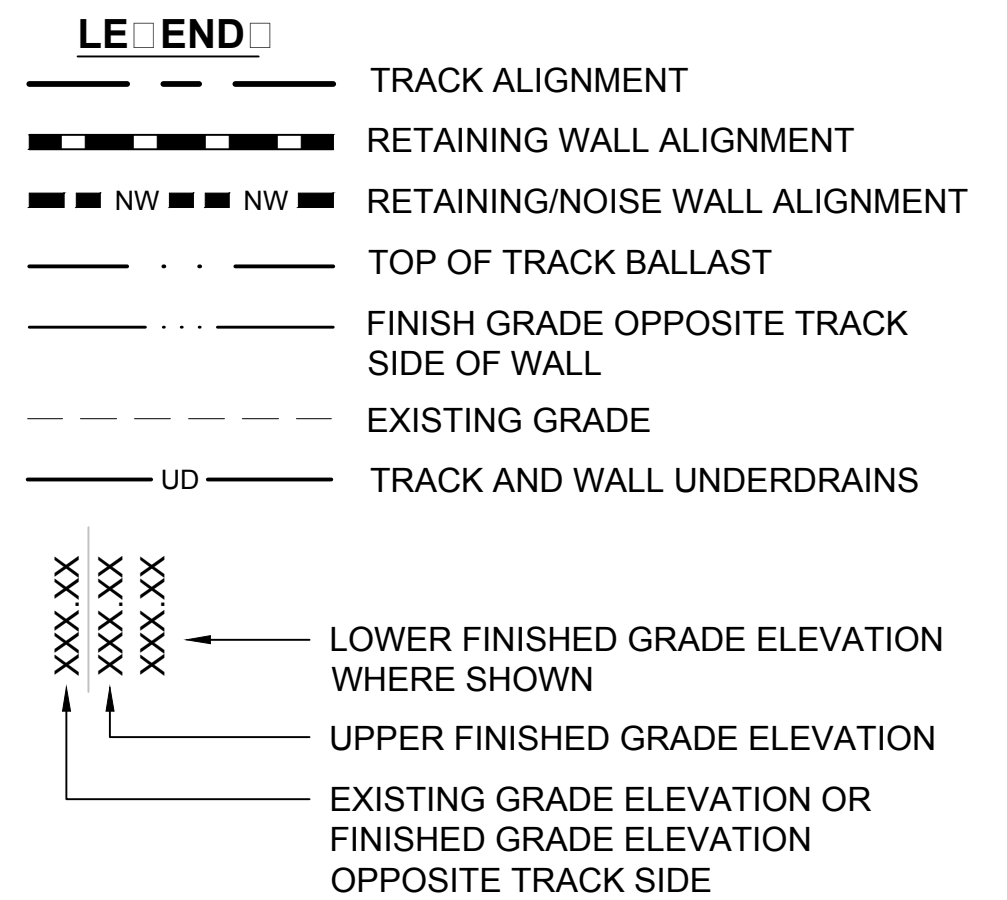
**WALL PLAN - NB 1588 25 TO NB 1590 25**



**SB WALL PROFILE - STA 229 54 TO STA 231 54**

**NOTES**

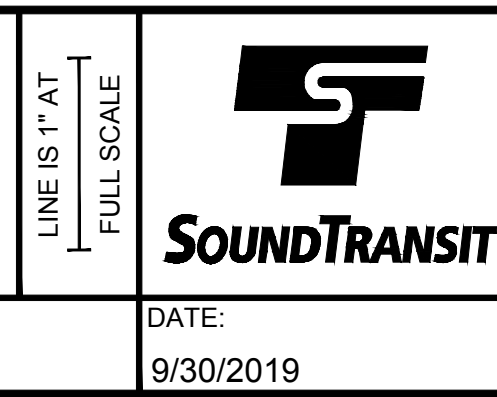
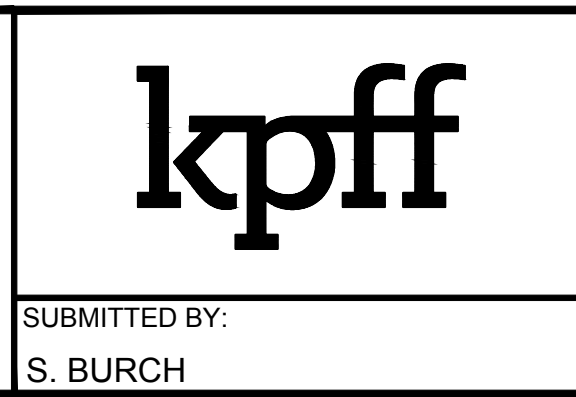
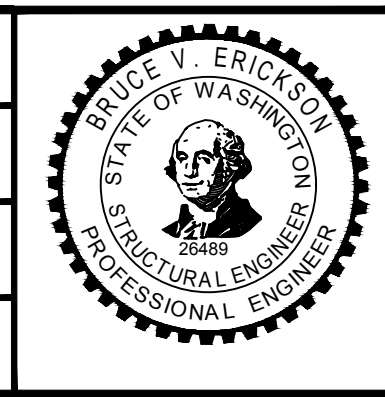
1. SEE DRAWING L85-SWP800 FOR TYPICAL WALL PLAN AND PROFILE NOTES.
2. SEE DRAWING L85-SWP864 FOR WALL GEOMETRY SCHEDULES.



11/09/20 | 4:04 PM | HAOYIS  
 C:\CAD\BIP\W\H\WEST\DM\863012\L200-L85-SWP863.DWG

DESIGNED BY:	D. EVANGER				
DRAWN BY:	H. SHI				
CHECKED BY:	B. ERICKSON				
APPROVED BY:	B. ERICKSON				
REV	DATE	DSN	CHK	APP	REVISION
RUD	12/07/20	JS	BE	BE	REV UNDER DEVELOPMENT - SHORELINE TAR WALL
0	09/30/19				ISSUED FOR CONTRACT / CO 002

DESIGNED BY:  
D. EVANGER  
 DRAWN BY:  
H. SHI  
 CHECKED BY:  
B. ERICKSON  
 APPROVED BY:  
B. ERICKSON



SCALE:  
1" = 10'  
 FILENAME:  
L200-L85-SWP863  
 CONTRACT No.:  
RTA / CN 0079-15C  
 DATE:  
9/30/2019

**LYNNWOOD LINK EXTENSION  
 CONTRACT L200**  
 NORTHGATE STATION TO NE 200TH STREET  
 STRUCTURES, WALLS  
 PLAN, PROFILES & SCHEDULES  
 NB 1588 25 TO NB 1590 25 SHT 1

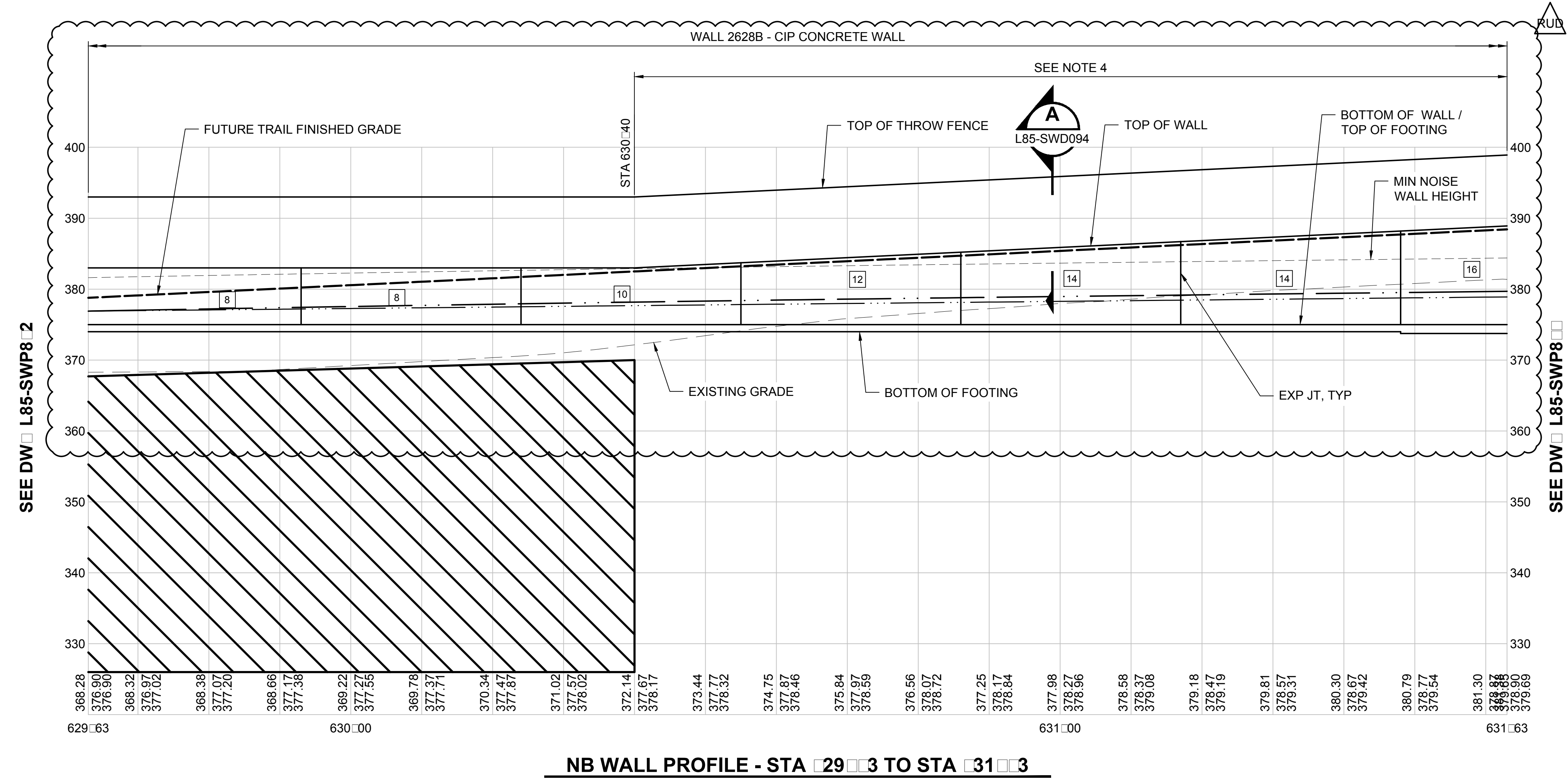
DRAWING No.:  
**L85-SWP8 3**  
 LOCATION ID:  
N16  
 SHEET No.:  
1582  
 REV:  
RUD



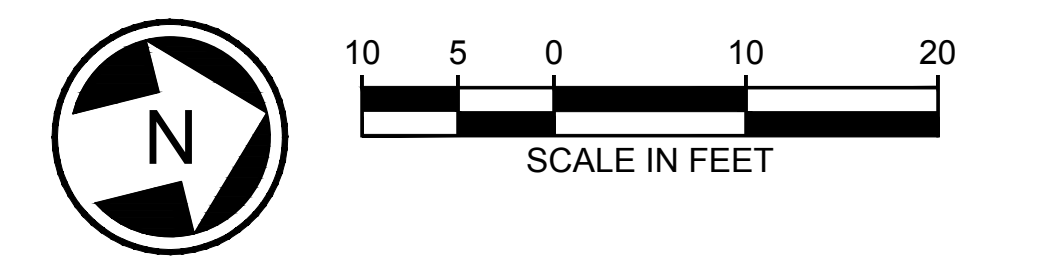
Xrefs:  
 xl200-G8-TB22x34  
 xl200-L85-KAP100  
 G8-SCAL-SWE26489  
 xl200-L85-KW100  
 xl200-L85-SWP901  
 xl200-L85-SW301  
 xl200-L85-KWP110  
 xl200-L85-KW101

NB WALL SCHEDULE		
VERTICAL GEOMETRY		
WALL STATION	TOP OF WALL ELEVATION	BOTTOM OF WALL ELEVATION
630+40.00	383.00	375.00

- NOTES**
- SEE DRAWING L85-SWP800 FOR TYPICAL WALL PLAN AND PROFILE NOTES.
  - SEE DRAWING L85-SWP863 FOR WALL PLAN.
  - INDICATES EXPOSED WALL HEIGHT, H, PER DRAWING L85-SWD041.
  - NO FORMLINER REQUIRED ON EAST (NON-TRACK) SIDE OF WALL OVER STATIONS INDICATED.

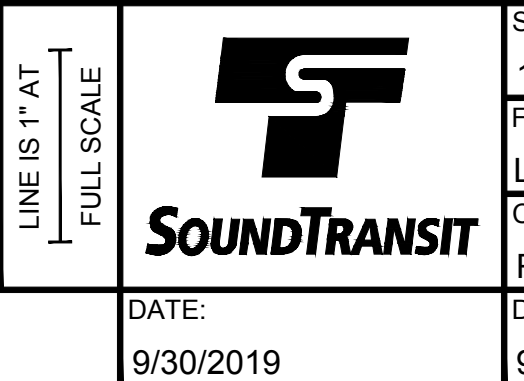
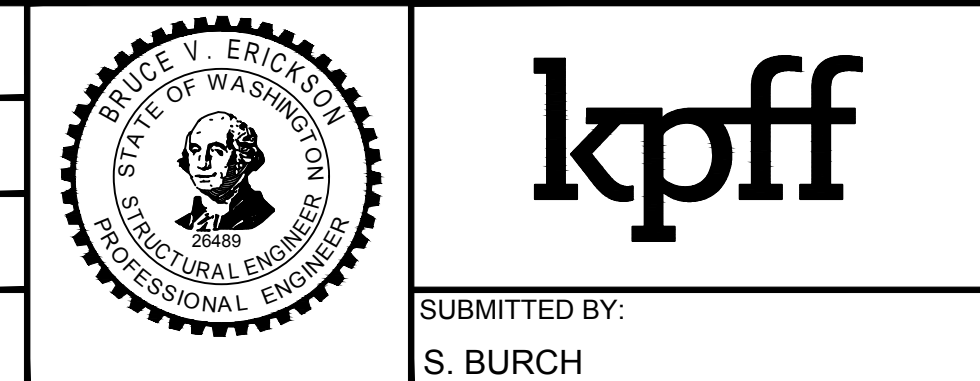
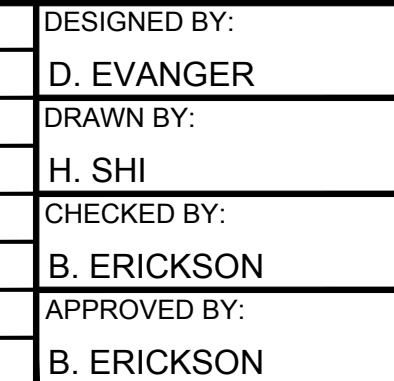


- LEGEND**
- TRACK ALIGNMENT
  - RETAINING WALL ALIGNMENT
  - RETAINING/NOISE WALL ALIGNMENT
  - TOP OF TRACK BALLAST
  - FINISH GRADE OPPOSITE TRACK SIDE OF WALL
  - EXISTING GRADE
  - TRACK AND WALL UNDERDRAINS
  - LOWER FINISHED GRADE ELEVATION WHERE SHOWN
  - UPPER FINISHED GRADE ELEVATION
  - EXISTING GRADE ELEVATION OR FINISHED GRADE ELEVATION OPPOSITE TRACK SIDE



**NB WALL PROFILE - STA 29+3 TO STA 31+3**

DESIGNED BY:	D. EVANGER
DRAWN BY:	H. SHI
CHECKED BY:	B. ERICKSON
APPROVED BY:	B. ERICKSON
REVISION	12/07/20 JS BE BE REV UNDER DEVELOPMENT-SHORELINE TAR WALL 09/30/19 ISSUED FOR CONTRACT / CO 002



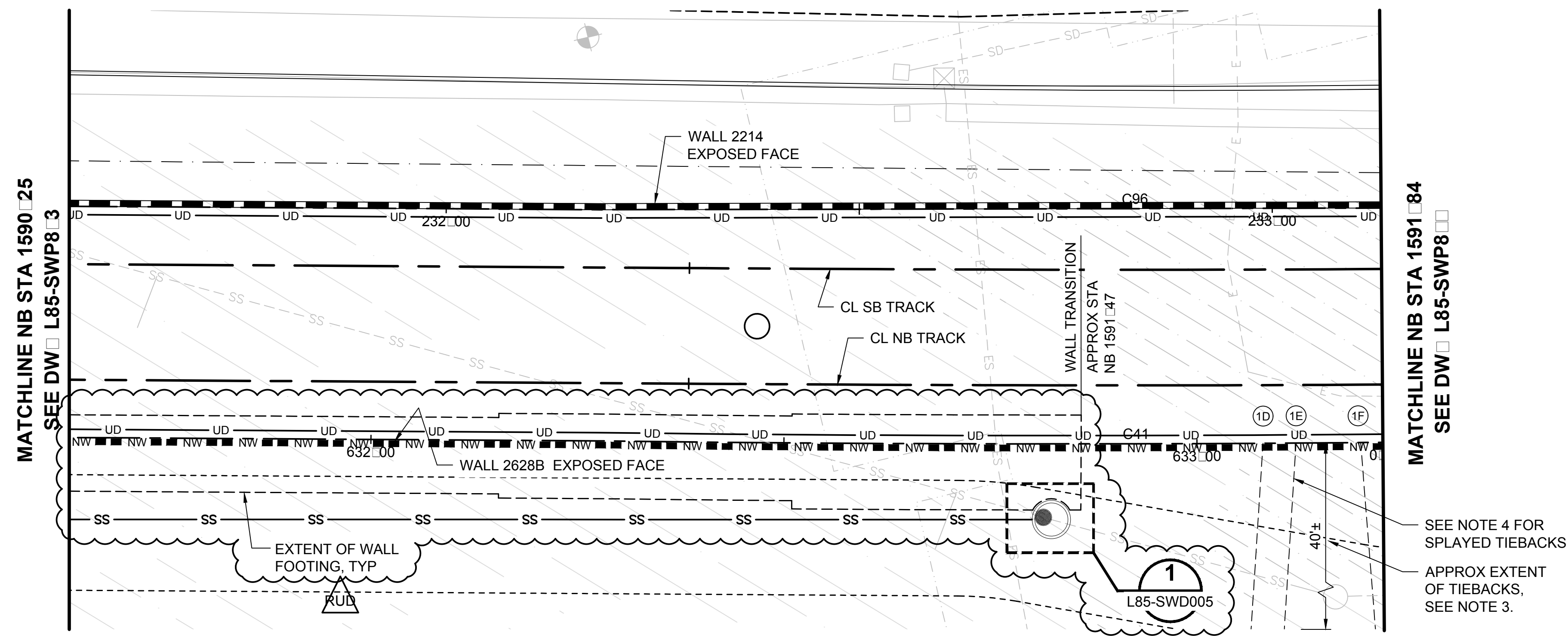
SCALE:  
 1" = 10'  
 FILENAME:  
 L200-L85-SWP864  
 CONTRACT No.:  
 RTA / CN 0079-15C  
 DATE:  
 9/30/2019

**LYNNWOOD LINK EXTENSION  
 CONTRACT L200**  
 NORTHGATE STATION TO NE 200TH STREET  
 STRUCTURES, WALLS  
 PLAN, PROFILES & SCHEDULES  
 NB 1588+25 TO NB 1590+25 SHT 2

DRAWING No.:	L85-SWP804
LOCATION ID:	N16
SHEET No.:	1583
REV:	RUD

12/03/20 9:21 AM | HAOYIS  
 C:\CADD\LIB\W\H\WEST\DM\863012\L200-L85-SWP864.DWG

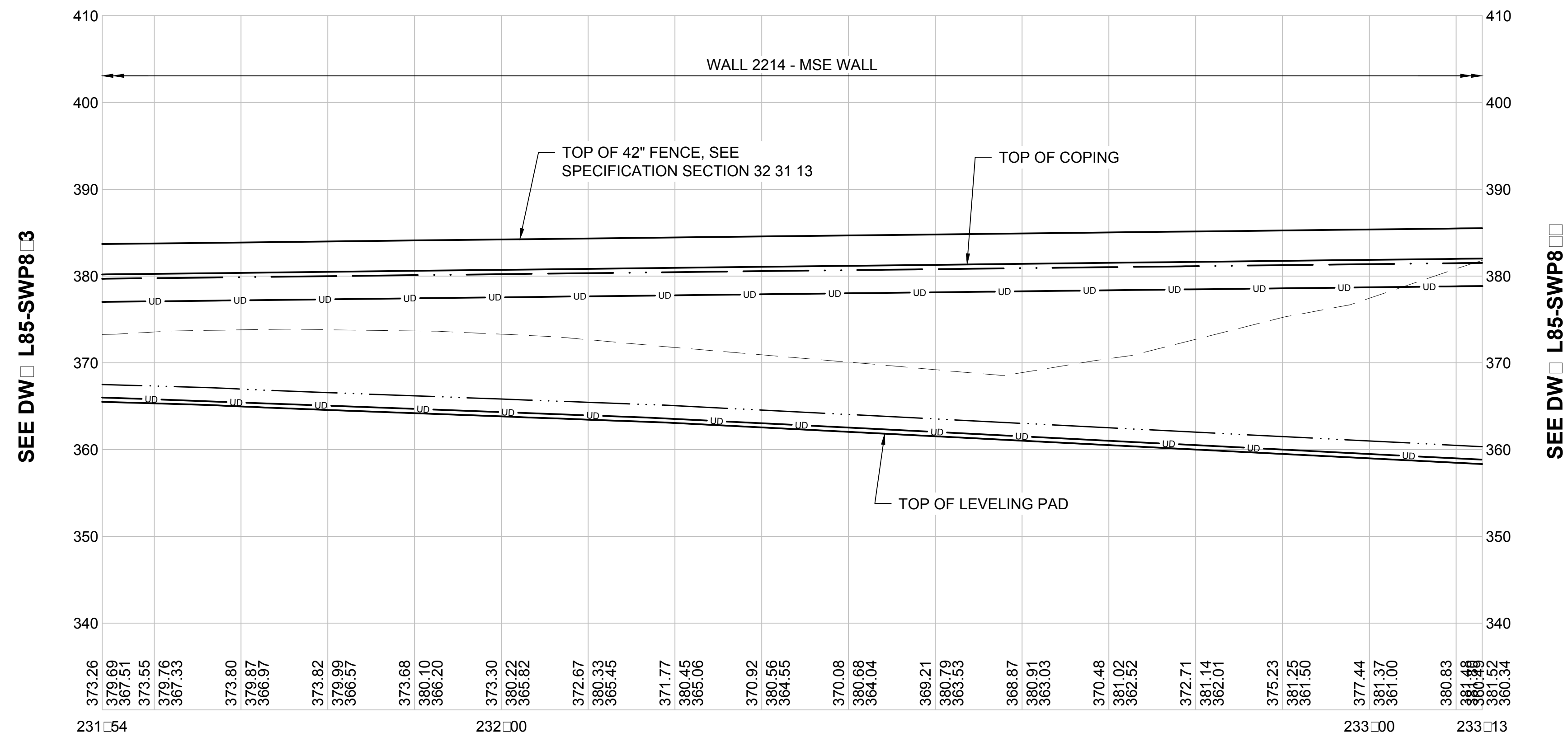
Xrefs:  
 XL200-GB-TB22x34  
 XL200-L85-CDP100  
 XL200-L85-CRP100  
 XL200-L85-KAP100  
 GB-SEAL-BVE26489  
 XL200-L85-KW100  
 XLLE-SHLN-VBP  
 XLLE-SHLN-VRX  
 XL200-L85-SWP901  
 XL200-L85-SW301  
 XL200-L85-KWP110  
 XL200-L85-JOP100  
 XL200-L85-KW101  
 XLLE-SHLN-CBP  
 XL200-L85-eUCP100  
 XL200-L85-eRPP100



**WALL PLAN - NB 1590 25 TO NB 1591 84**

**NOTES**

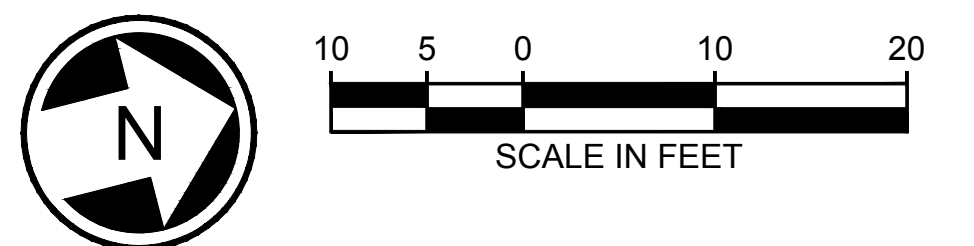
1. SEE DRAWING L85-SWP800 FOR TYPICAL WALL PLAN AND PROFILE NOTES.
2. SEE DRAWING L85-SWP866 FOR WALL GEOMETRY SCHEDULES.
3. TIEBACK LENGTHS VARY ALONG RANGE SHOWN. APPROXIMATE LENGTH INDICATED REPRESENTS MAXIMUM LENGTH OVER THAT RANGE. LENGTHS ARE NOT DRAWN TO SCALE.
4. PILE 1D, 1E, AND 1F TIEBACKS TO BE SPLAYED 2 DEG HORIZONTALLY AS SHOWN TO AVOID EXISTING MANHOLE.



**SB WALL PROFILE - STA 231 54 TO STA 233 13**

**LE END**

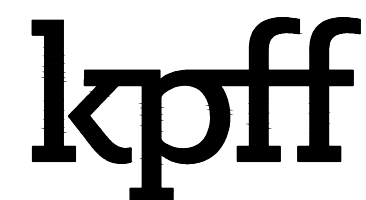
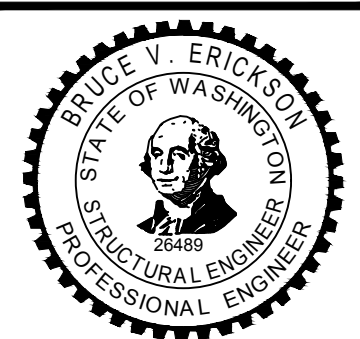
- TRACK ALIGNMENT
- RETAINING WALL ALIGNMENT
- RETAINING/NOISE WALL ALIGNMENT
- TOP OF TRACK BALLAST
- FINISH GRADE OPPOSITE TRACK SIDE OF WALL
- EXISTING GRADE
- TRACK AND WALL UNDERDRAINS
- LOWER FINISHED GRADE ELEVATION WHERE SHOWN
- UPPER FINISHED GRADE ELEVATION
- EXISTING GRADE ELEVATION OR FINISHED GRADE ELEVATION OPPOSITE TRACK SIDE



11/11/20 | 1:36 PM | HAOYIS  
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RUD	12/07/20	JS	BE	BE	REV UNDER DEVELOPMENT - SHORELINE TAR WALL
A	02/19/20	DE	BE	BE	CNWD #033
0	09/30/19				ISSUED FOR CONTRACT / CO 002
No.	DATE	DSN	CHK	APP	REVISION

DESIGNED BY:  
D. EVANGER  
 DRAWN BY:  
H. SHI  
 CHECKED BY:  
B. ERICKSON  
 APPROVED BY:  
B. ERICKSON



LINE IS 1" AT FULL SCALE



SCALE:  
1" = 10'  
 FILENAME:  
L200-L85-SWP865  
 CONTRACT No.:  
RTA / CN 0079-15C  
 DATE:  
9/30/2019

**LYNNWOOD LINK EXTENSION  
 CONTRACT L200**  
 NORTHGATE STATION TO NE 200TH STREET  
 STRUCTURES, WALLS  
 PLAN, PROFILES & SCHEDULES  
 NB 1590 25 TO NB 1591 84 SHT 1

DRAWING No.:  
**L85-SWP8 5**  
 LOCATION ID:  
N16  
 SHEET No.:  
1584  
 REV:  
RUD



Xrefs:  
 xl200-GB-TB22x34  
 xl200-L85-KAP100  
 GB-SCAL-SWE26489  
 xl200-L85-KW100  
 xl200-L85-SWP901  
 xl200-L85-KWP110  
 xl200-L85-KW101  
 xl200-L85-SW301

SB WALL SCHEDULE					
HORIZONTAL GEOMETRY					
POINT	WALL STATION	CONTROL LINE NORTH COORDINATE	CONTROL LINE EAST COORDINATE	DATA	OFFSET FROM CONTROL LINE TO WORKLINE
PC	232+18.69	375881.80	1372599.64	-	-
PI	232+83.40	375944.72	1372614.76	▲ = 000°02'44" LC = 129.40 R = 163068.78 T = 64.70	-

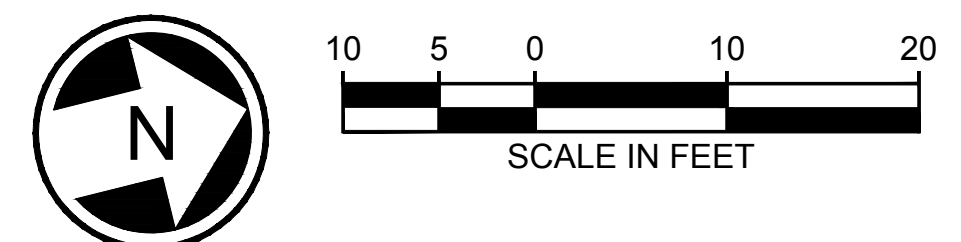
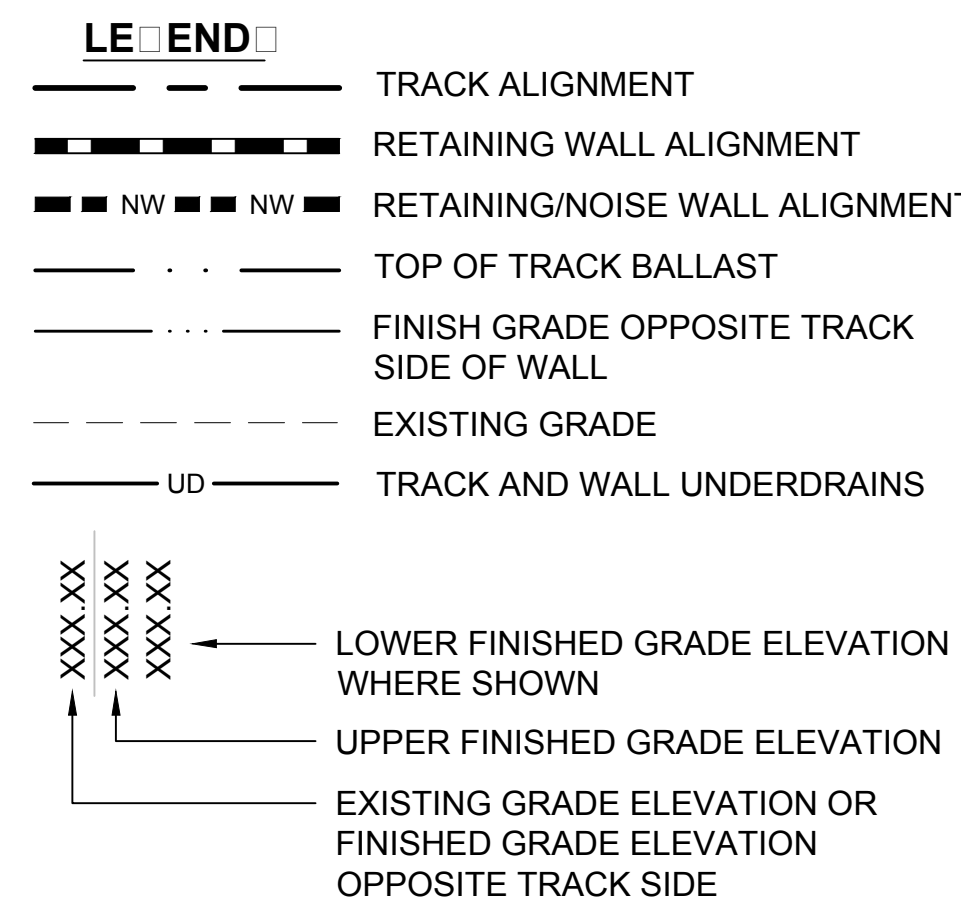
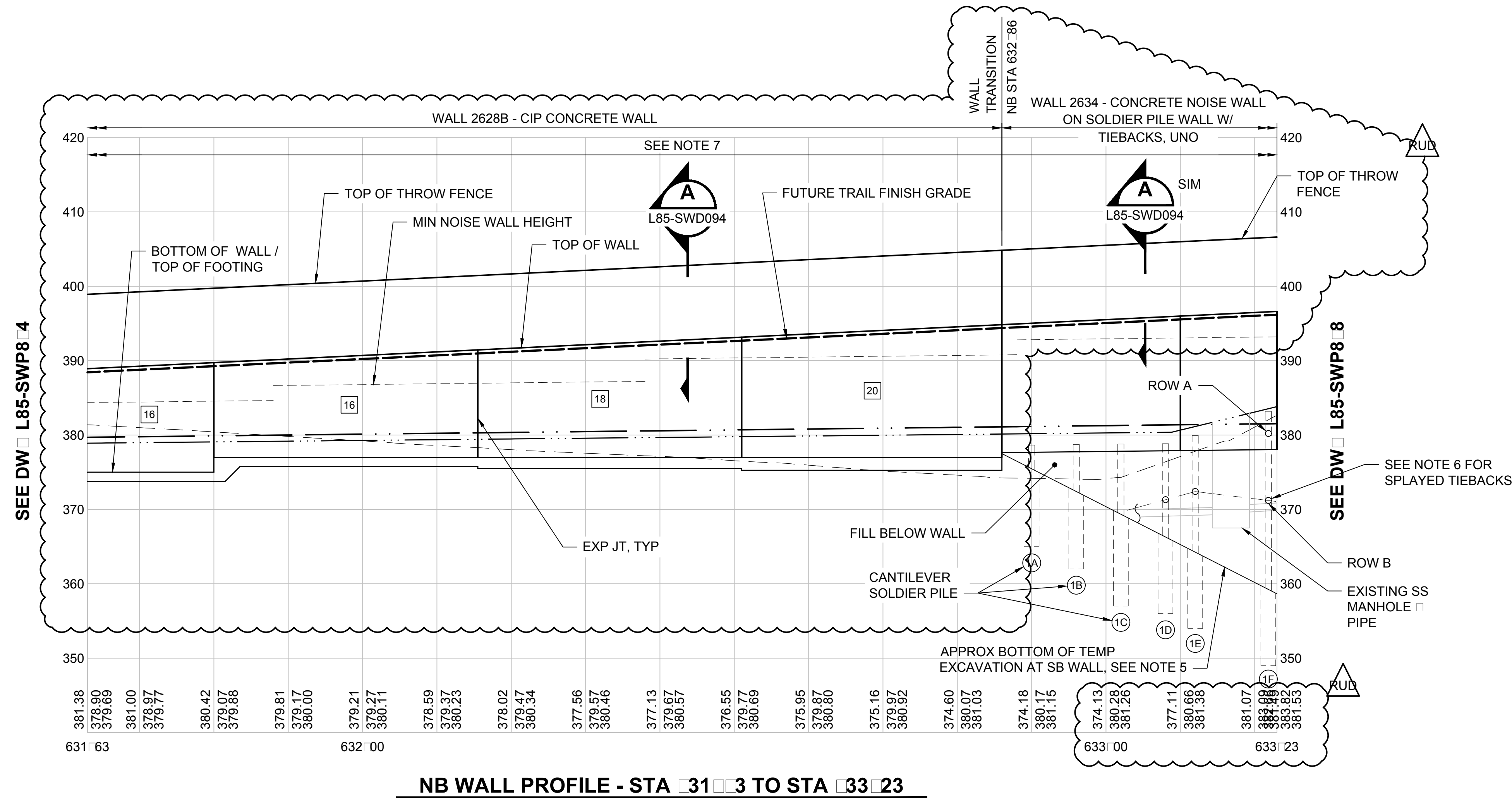
NB WALL SCHEDULE					
HORIZONTAL GEOMETRY					
POINT	WALL STATION	CONTROL LINE NORTH COORDINATE	CONTROL LINE EAST COORDINATE	DATA	OFFSET FROM CONTROL LINE TO WORKLINE
PC	632+27.42	375874.84	1372626.76	-	-
PI	632+74.97	375920.90	1372638.57	▲ = 001°17'39" LC = 95.10 R = 4210.03 T = 47.55	-
PT	633+22.52	375967.22	1372649.33	-	-
WALL TRANSITION	632+86.00	375931.68	1372640.91	-	0'-0" LEFT 0'-0" RIGHT

VERTICAL GEOMETRY		
WALL STATION	TOP OF WALL ELEVATION	BOTTOM OF WALL ELEVATION
631+80.00	-	375.00 LEFT 377.00 RIGHT
632+86.00	394.84	377.00 LEFT 377.63 RIGHT

**NOTES**

- SEE DRAWING L85-SWP800 FOR TYPICAL WALL PLAN AND PROFILE NOTES.
- SEE DRAWING L85-SWP865 FOR WALL PLAN.
- INDICATES EXPOSED WALL HEIGHT, H, PER DRAWING L85-SWD041.
- HORIZONTAL WALL GEOMETRY PROVIDED FOR CURVED WALL SEQUENCES DOES NOT REPRESENT TANGENCY BETWEEN ADJACENT SEGMENTS. CURVE SEGMENTS ARE NOT TANGENT TO ADJACENT SEGMENTS.
- SOLDIER PILE WALL DESIGNED FOR ASSUMED DEPTH OF TEMPORARY EXCAVATION SHOWN. CONTRACTOR TO NOTIFY ENGINEER IF REQUIRED TEMPORARY EXCAVATION EXCEEDS DEPTH SHOWN ON PROFILE.
- PILE 1D, 1E, AND 1F TIEBACKS TO BE SPLAYED 2 DEG HORIZONTALLY AS SHOWN ON SHEET L85-SWP865 WALL PLAN TO AVOID EXISTING MANHOLE.
- NO FORMLINER REQUIRED ON EAST (NON-TRACK) SIDE OF WALL OVER STATIONS INDICATED.

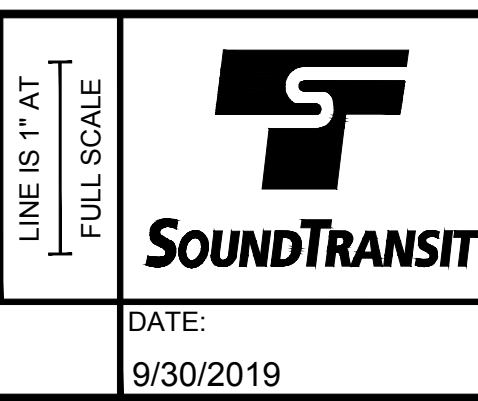
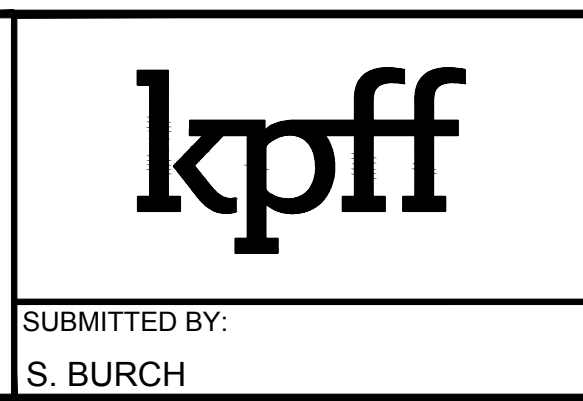
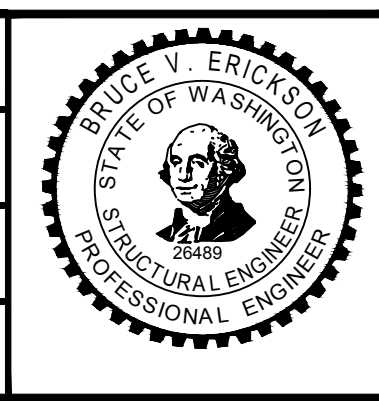


**NB WALL PROFILE - STA 31+3 TO STA 33+23**

12/15/20 | 10:25 AM | HA0YIS  
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No.	DATE	DSN	CHK	APP	REVISION
RUD	12/07/20	JS	BE	BE	REV UNDER DEVELOPMENT-SHORELINE TAR WALL
C	02/19/20	DE	BE	BE	CNWD #033
B	01/24/20	DE	BE	BE	PR #024
A	01/17/20	DE	BE	BE	PR #021
0	09/30/19				ISSUED FOR CONTRACT / CO 002

DESIGNED BY:  
D. EVANGER  
 DRAWN BY:  
H. SHI  
 CHECKED BY:  
B. ERICKSON  
 APPROVED BY:  
B. ERICKSON



SCALE:  
1" = 10'  
 FILENAME:  
L200-L85-SWP866  
 CONTRACT No.:  
RTA / CN 0079-15C  
 DATE:  
9/30/2019

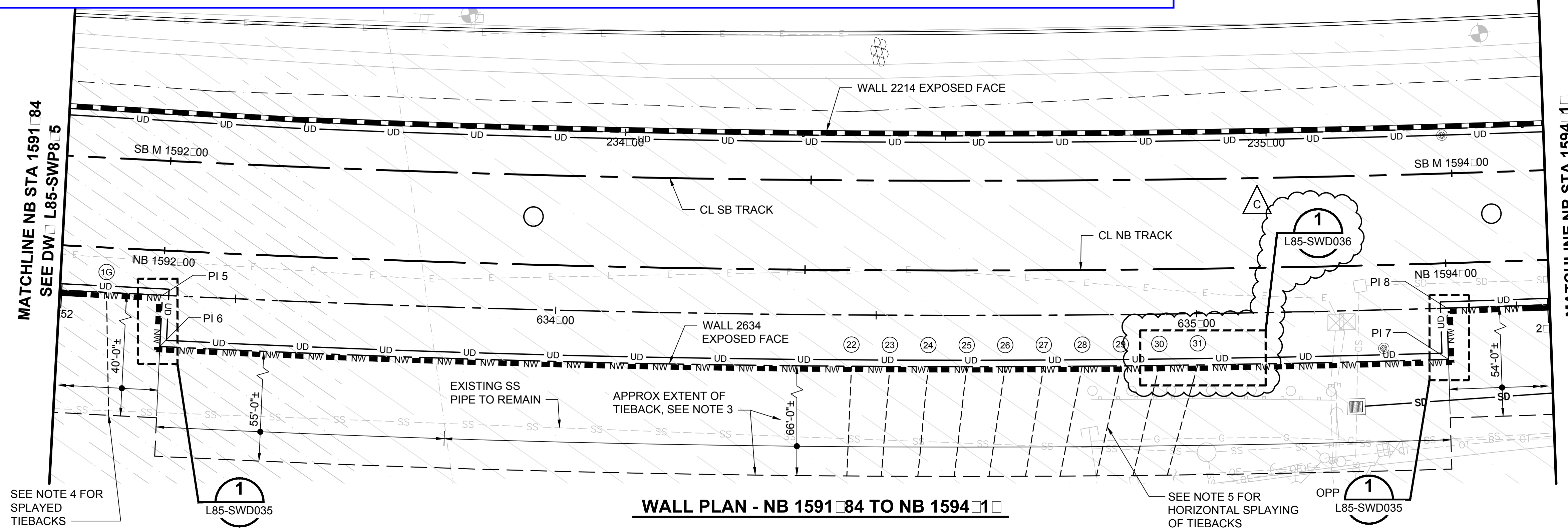
**LYNNWOOD LINK EXTENSION  
 CONTRACT L200**  
 NORTHGATE STATION TO NE 200TH STREET  
 STRUCTURES, WALLS  
 PLAN, PROFILES & SCHEDULES  
 NB 1590+25 TO NB 1591+84 SHT 2

DRAWING No.:  
**L85-SWP8**  
 LOCATION ID:  
N16  
 SHEET No.:  
1585  
 REV:  
RUD

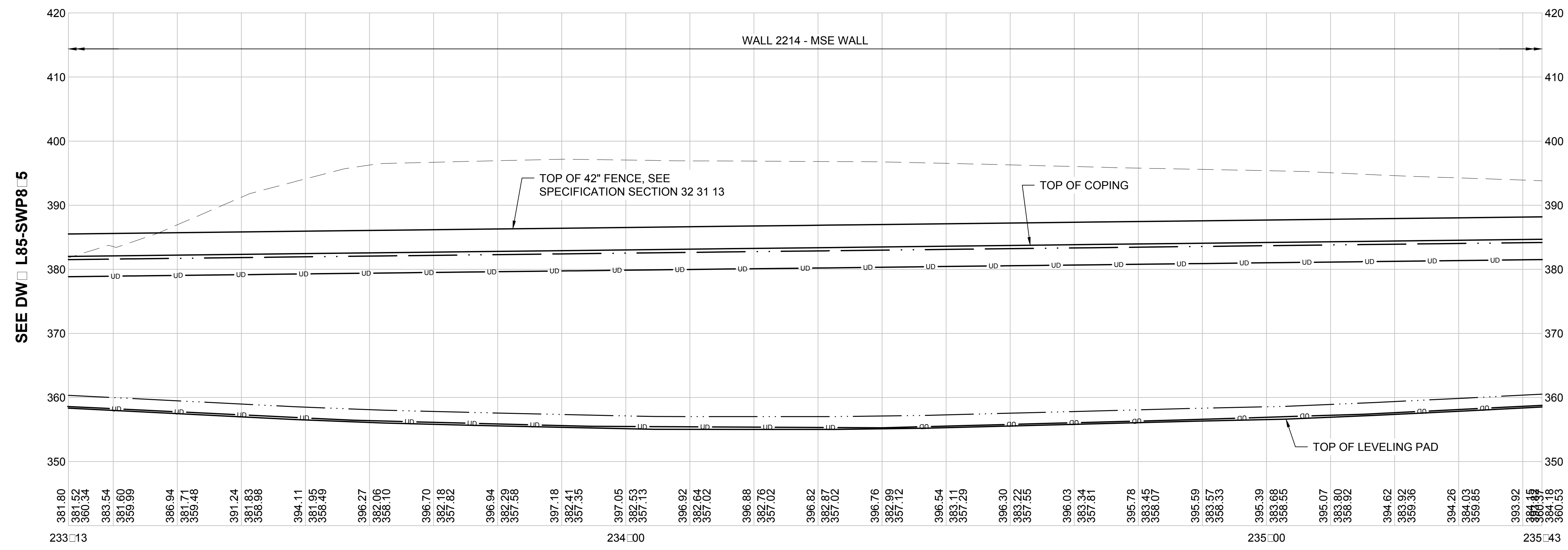


# NO CHANGES TO SHEET - INCLUDED FOR REFERENCE

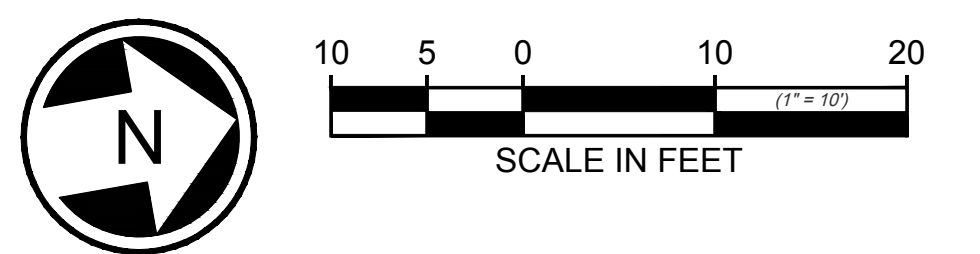
Xrefs:  
 XL200-GB-TB22x34  
 XL200-L85-CDP100  
 XL200-L85-CRP100  
 XL200-L85-KAP100  
 GB-SEAL-BVE26489  
 XL200-L85-KW100  
 XLLE-SHLN-YEP  
 XLLE-SHLN-VRX  
 XL200-L85-SWP901  
 XL200-L85-SW301  
 XL200-L85-KWP110  
 XL200-L85-JOP100  
 XL200-L85-KW101  
 XLLE-SHLN-CSP  
 XL200-L85-eRPP100



- NOTES**
- SEE DRAWING L85-SWP800 FOR TYPICAL WALL PLAN AND PROFILE NOTES.
  - SEE DRAWING L85-SWP868 FOR WALL GEOMETRY SCHEDULES.
  - TIEBACK LENGTHS VARY ALONG RANGE SHOWN. APPROXIMATE LENGTH INDICATED REPRESENTS MAXIMUM LENGTH OVER THAT RANGE. LENGTHS ARE NOT DRAWN TO SCALE.
  - PILE 1G TIEBACKS TO BE SPLAYED 2 DEG HORIZONTALLY AS SHOWN TO AVOID EXISTING MANHOLE.
  - PILE 31 TIEBACKS TO BE SPLAYED 15 DEGREES HORIZONTALLY AS SHOWN TO AVOID EXISTING MANHOLE. THE HORIZONTAL SPLAY OF PILE 22-30 TIEBACKS TO BE DECREASED LINEARLY SUCH THAT MINIMUM 4 FEET DISTANCE IS PROVIDED BETWEEN ADJACENT PILE TIEBACKS.



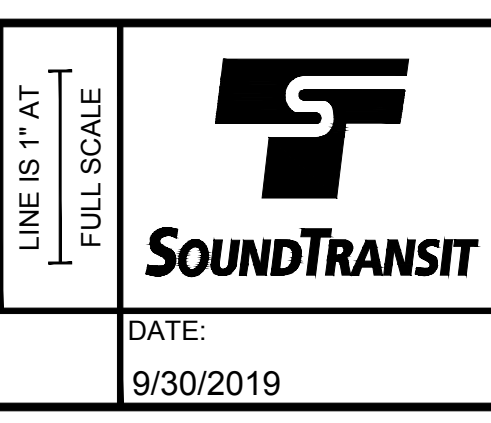
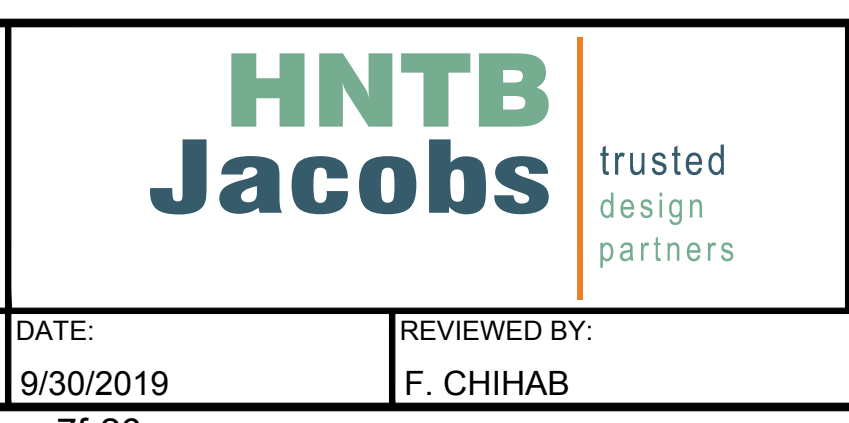
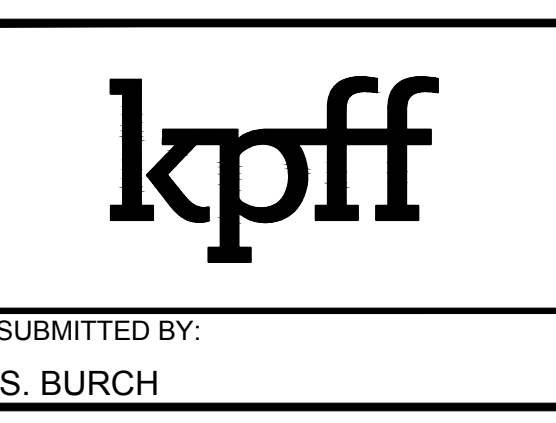
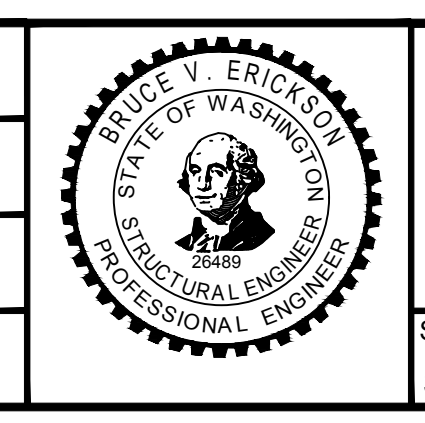
- LEGEND**
- TRACK ALIGNMENT
  - RETAINING WALL ALIGNMENT
  - RETAINING/NOISE WALL ALIGNMENT
  - TOP OF TRACK BALLAST
  - FINISH GRADE OPPOSITE TRACK SIDE OF WALL
  - EXISTING GRADE
  - TRACK AND WALL UNDERDRAINS
  - LOWER FINISHED GRADE ELEVATION WHERE SHOWN
  - UPPER FINISHED GRADE ELEVATION
  - EXISTING GRADE ELEVATION OR FINISHED GRADE ELEVATION OPPOSITE TRACK SIDE



03/19/20 | 11:43 AM | HA0YIS  
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No.	DATE	DSN	CHK	APP	REVISION
C	03/23/20	DE	BE	BE	CNWD #043 REV 1
B	02/26/20	DE	BE	BE	CNWD #043
A	02/19/20	DE	BE	BE	CNWD #033
0	09/30/19				ISSUED FOR CONTRACT / CO 002

DESIGNED BY:  
D. EVANGER  
 DRAWN BY:  
H. SHI  
 CHECKED BY:  
B. ERICKSON  
 APPROVED BY:  
B. ERICKSON



SCALE:  
1" = 10'  
 FILENAME:  
L200-L85-SWP867  
 CONTRACT No.:  
RTA / CN 0079-15C  
 DATE:  
9/30/2019

**LYNNWOOD LINK EXTENSION  
 CONTRACT L200**  
 NORTHGATE STATION TO NE 200TH STREET  
 STRUCTURES, WALLS  
 PLAN, PROFILES & SCHEDULES  
 NB 1591 84 TO NB 1594 16 SHT 1

DRAWING No.:  
**L85-SWP8**  
 LOCATION ID:  
N16  
 SHEET No.:  
1586  
 REV:  
C



Xrefs:  
 XL200-G8-TB22x34  
 XL200-L85-KAP100  
 G8-SCAL-SW226489  
 XL200-L85-KW100  
 XL200-L85-SWP901  
 XL200-L85-KWP110  
 XL200-L85-KW101  
 XL200-L85-SW301

NB WALL SCHEDULE					
HORIZONTAL GEOMETRY					
POINT	WALL STATION	CONTROL LINE NORTH COORDINATE	CONTROL LINE EAST COORDINATE	DATA	OFFSET FROM CONTROL LINE TO WORKLINE
WALL TRANSITION	0+44.20	375967.51	1372649.40	-	0'-0"
PI 5	0+59.93	375982.84	1372652.93	-	0'-0"
PI 6/PC	0+67.93	375981.05	1372660.73	-	8'-0"
PI	1+68.58	376079.15	1372683.27	Δ = 004°35'53" LC = 201.20 R = 2507.04 T = 100.65	-
PI 7/PT	2+69.13	376178.74	1372697.87	-	8'-0"
PI 8	2+77.13	376179.90	1372689.96	-	0'-0"
WALL TRANSITION	2+92.86	376195.47	1372692.19	-	0'-0"

NB VERTICAL GEOMETRY		
WALL STATION	TOP OF WALL ELEVATION	BOTTOM OF WALL ELEVATION
WALL TRANSITION / 0+44.20	396.62	-
PI 5 = 0+59.93	397.38 LEFT 407.50 RIGHT	-
1+01.00	407.50 LEFT 411.50 RIGHT	-
1+09.00	411.50 LEFT 415.50 RIGHT	-
1+17.00	415.50 LEFT 419.50 RIGHT	-
1+97.00	419.50 LEFT 417.50 RIGHT	-

**NOTES**

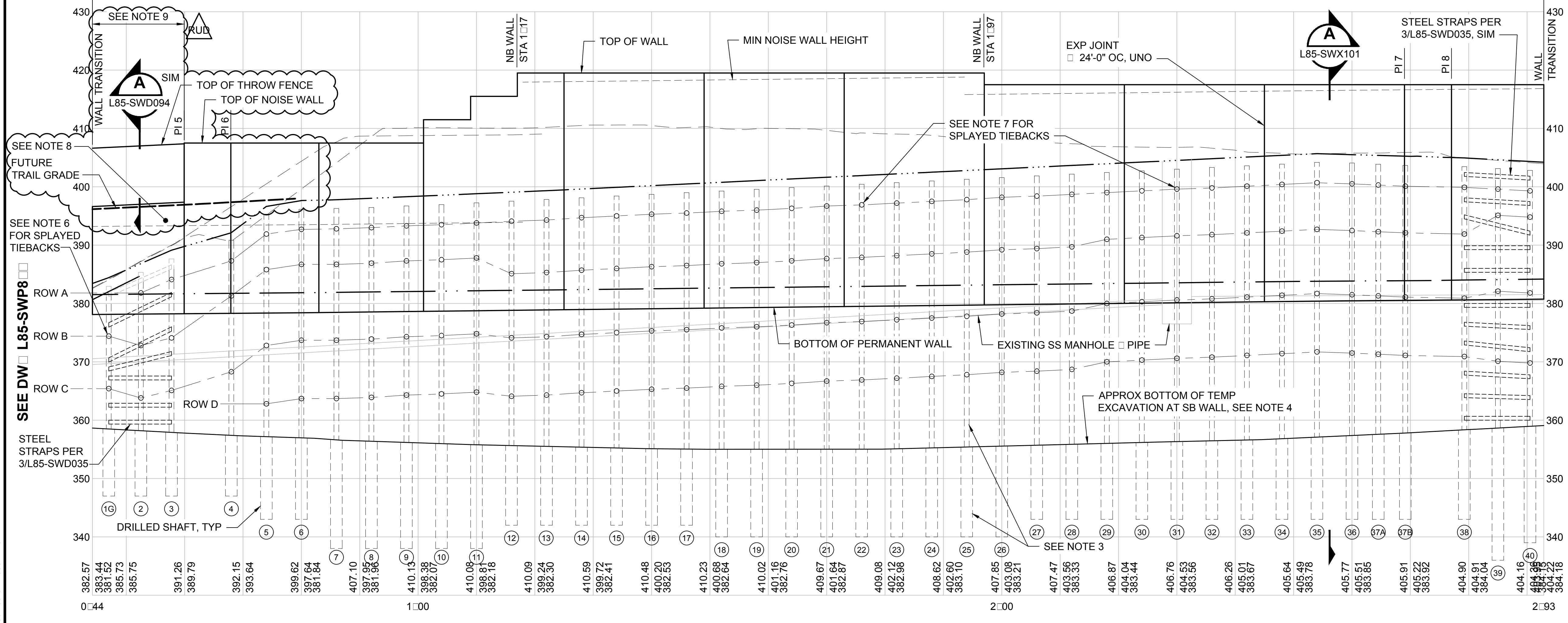
- SEE DRAWING L85-SWP800 FOR TYPICAL WALL PLAN AND PROFILE NOTES.
- SEE DRAWING L85-SWP867 FOR WALL PLAN.
- SEE DRAWING L85-SWS107 AND L85-SWS108 FOR SOLDIER PILES, SHAFT, AND TIEBACK SCHEDULE.
- SOLDIER PILE WALL DESIGNED FOR ASSUMED DEPTH OF TEMPORARY EXCAVATION SHOWN. CONTRACTOR TO NOTIFY ENGINEER IF REQUIRED TEMPORARY EXCAVATION EXCEEDS DEPTH SHOWN ON PROFILE.
- HORIZONTAL WALL GEOMETRY PROVIDED FOR CURVED WALL SEQUENCES DOES NOT REPRESENT TANGENCY BETWEEN ADJACENT SEGMENTS. CURVE SEGMENTS ARE NOT TANGENT TO ADJACENT SEGMENTS.
- PILE 1G TIEBACKS TO BE SPLAYED 2 DEGREES HORIZONTALLY AS SHOWN ON SHEET L85-SWP867 WALL PLAN TO AVOID EXISTING MANHOLE.
- PILE 31 TIEBACKS TO BE SPLAYED 15 DEGREES HORIZONTALLY AS SHOWN ON SHEET L85-SWP867 WALL PLAN TO AVOID EXISTING MANHOLE. THE HORIZONTAL SPLAY OF PILE 22-30 TIEBACKS TO BE DECREASED LINEARLY SUCH THAT MINIMUM 4 FEET DISTANCE IS PROVIDED BETWEEN ADJACENT PILE TIEBACKS.
- PROVIDE #5 9" VERTICAL NW REINF EACH FACE FROM WALL TRANSITION TO PI 5.
- NO FORMLINER REQUIRED ON EAST (NON-TRACK) SIDE OF WALL OVER STATIONS INDICATED.

**LEGEND**

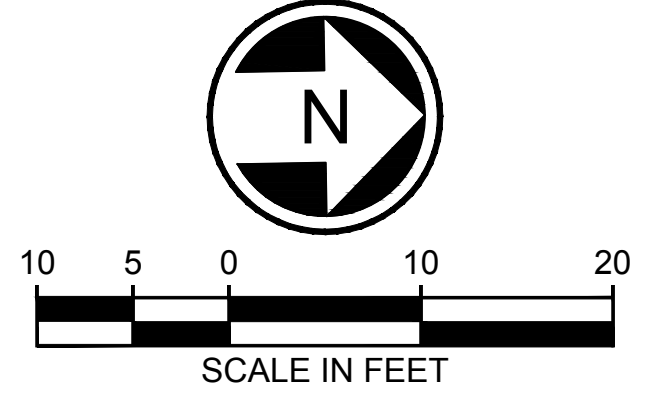
- TRACK ALIGNMENT
- RETAINING WALL ALIGNMENT
- RETAINING/NOISE WALL ALIGNMENT
- TOP OF TRACK BALLAST
- FINISH GRADE OPPOSITE TRACK SIDE OF WALL
- EXISTING GRADE
- TRACK AND WALL UNDERDRAINS
- LOWER FINISHED GRADE ELEVATION WHERE SHOWN
- UPPER FINISHED GRADE ELEVATION
- EXISTING GRADE ELEVATION OR FINISHED GRADE ELEVATION OPPOSITE TRACK SIDE

SB WALL SCHEDULE					
HORIZONTAL GEOMETRY					
POINT	WALL STATION	CONTROL LINE NORTH COORDINATE	CONTROL LINE EAST COORDINATE	DATA	OFFSET FROM CONTROL LINE TO WORKLINE
PT/PC	233+48.10	376007.64	1372629.83	-	-

WALL 2634 - CONCRETE NOISE WALL ON SOLDIER PILE WALL W/ TIEBACKS

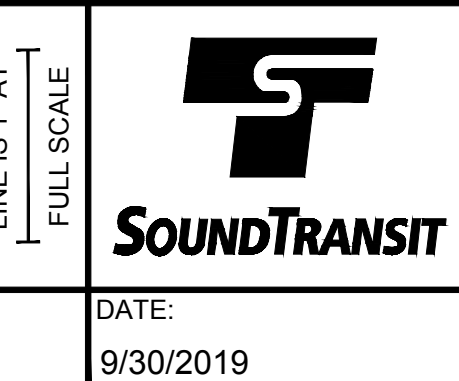


NB WALL PROFILE - STA 0+50 TO STA 2+8



No.	DATE	DSN	CHK	APP	REVISION
RUD	12/07/20	JS	BE	BE	REV UNDER DEVELOPMENT-SHORELINE TAR WALL
C	02/26/20	DE	BE	BE	CNWD #043
B	02/19/20	DE	BE	BE	CNWD #033
A	01/24/20	DE	BE	BE	PR #024
0	09/30/19				ISSUED FOR CONTRACT / CO 002

DESIGNED BY:  
D. EVANGER  
 DRAWN BY:  
H. SHI  
 CHECKED BY:  
B. ERICKSON  
 APPROVED BY:  
B. ERICKSON



SCALE:  
1" = 10'  
 FILENAME:  
L200-L85-SWP868  
 CONTRACT No.:  
RTA / CN 0079-15C  
 DATE:  
9/30/2019

**LYNNWOOD LINK EXTENSION  
 CONTRACT L200**  
 NORTHGATE STATION TO NE 200TH STREET  
 STRUCTURES, WALLS  
 PLAN, PROFILES & SCHEDULES  
 NB 1591+84 TO NB 1594+16 SHT 2

DRAWING No.:  
**L85-SWP808**  
 LOCATION ID:  
N16  
 SHEET No.:  
1587  
 REV:  
RUD

12/03/20 | 9:42 AM | HAOYIS  
 C:\CADD\BIP\W\H\WEST\DM\863012\L200-L85-SWP868.DWG

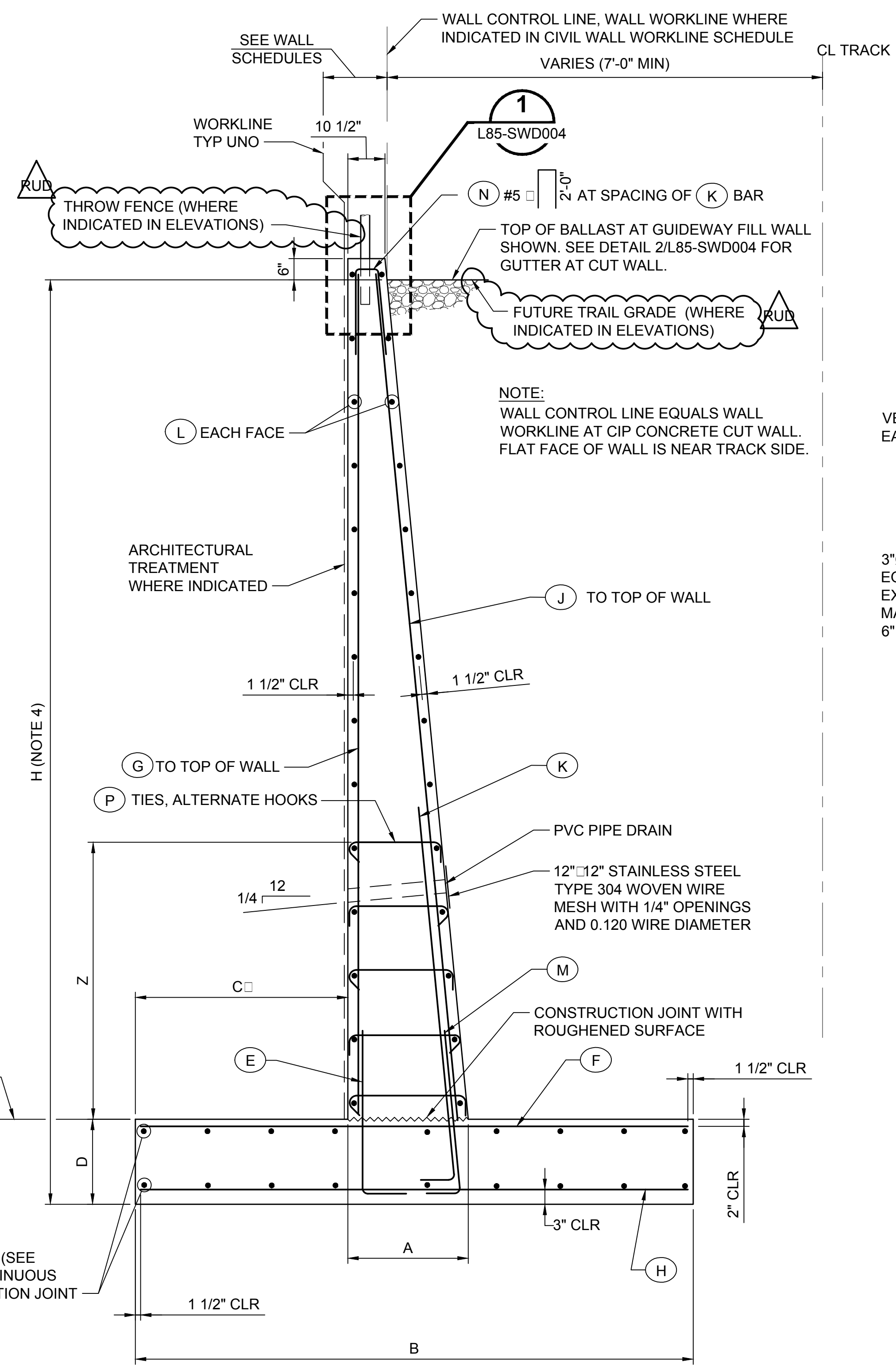


Xrefs:  
 XL200-GB-TB22x34  
 XLLE-STD-SWD003  
 GB-SEAL-BVC26489

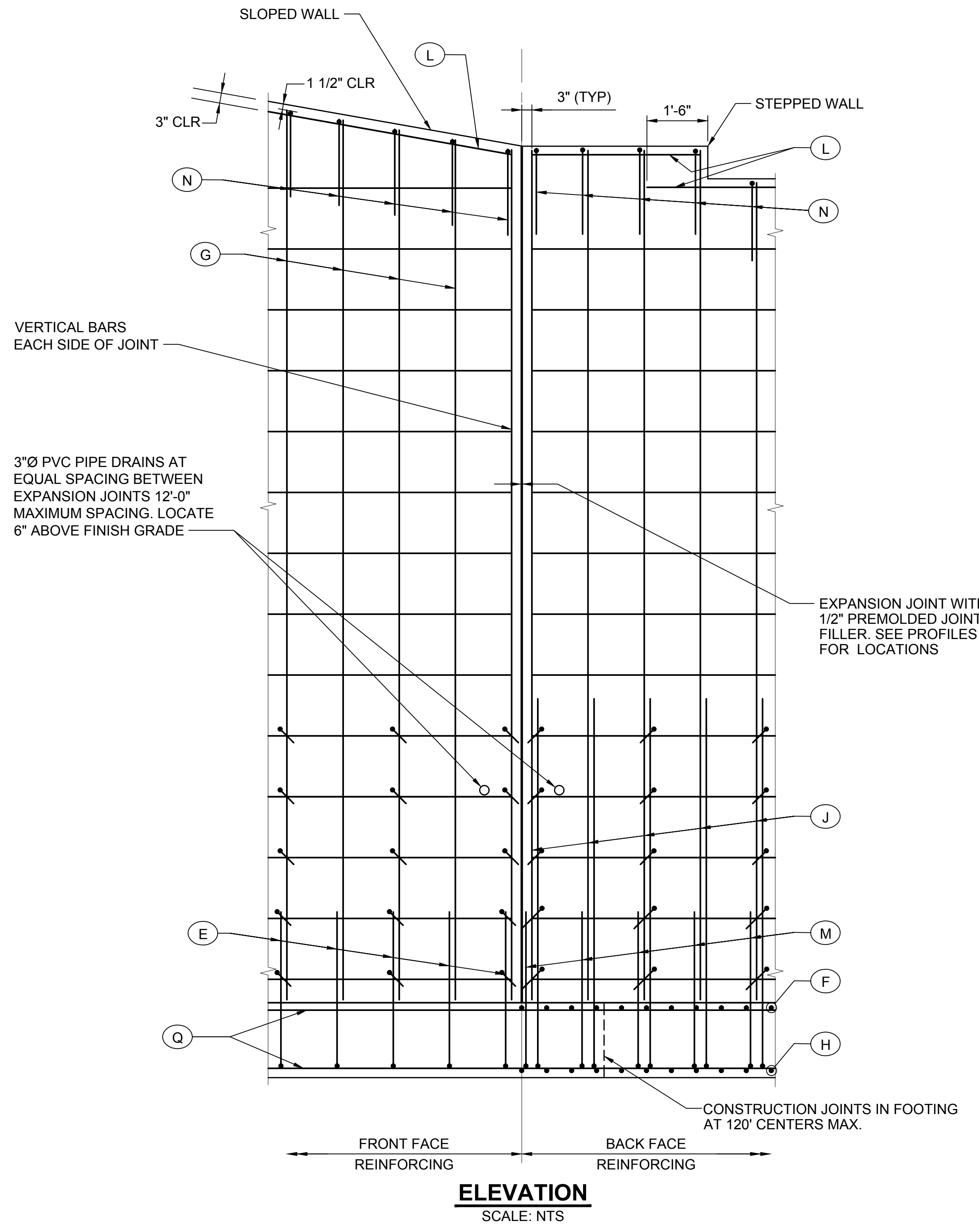
**GENERAL NOTES:**

- FOR BACKFILL REQUIREMENTS, SEE DRAWING L85-SWD005.
- FOR TYPICAL WALL AND FOOTING DETAILS SEE DRAWING L85-SWD004.
- CONCRETE SECTIONS BETWEEN EXPANSION JOINTS SHALL BE PLACED SEPARATELY WITH A 24 HOUR MINIMUM PERIOD BEFORE PLACING CONCRETE IN THE ADJACENT SECTION.
- DESIGN HEIGHT H, CONSTRUCT TO PROJECT PLAN GEOMETRY. SEE TABLE 1 ON DRAWING L85-SWD003 FOR WALL SCHEDULES.
- CONTINUOUSLY COAT ALL WALL SURFACES PERMANENTLY EXPOSED TO VIEW TO A MINIMUM OF 1 FOOT BELOW FINAL GRADE WITH PIGMENTED SEALER. SEE DRAWING L85-SWD082 FOR EXTENTS OF ANTI-GRAFFITI COATING.
- ALIGN FACE OF CIP NOISE WALL WITH FLAT FACE OF PRECAST PANELS.

L200 CIVIL WALL WORKLINE SCHEDULE	
WALL	WORKLINE LOCATION
R03-1R	BACK FACE □ FINISH GRADE
R21-2L	BACK FACE □ FINISH GRADE
N15-1	FRONT FACE
R26-1R	FRONT FACE
NW 1554	SEE NOTE 6



**TYPICAL SECTION**  
SCALE: NTS

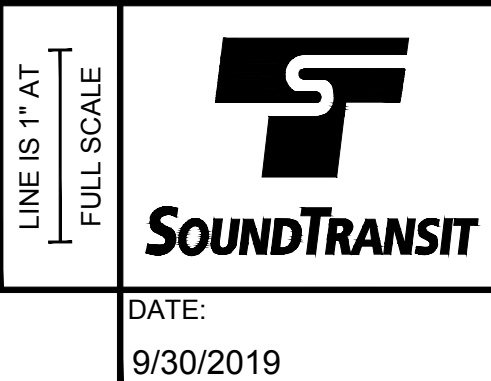
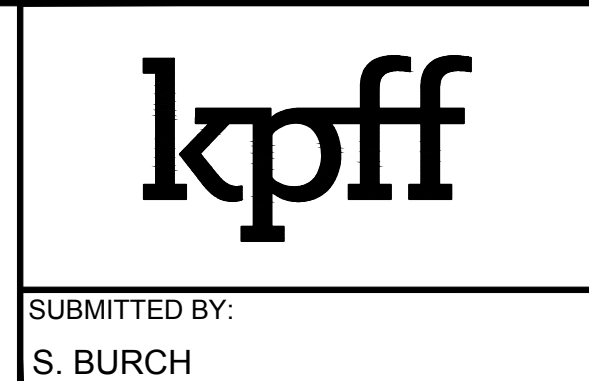
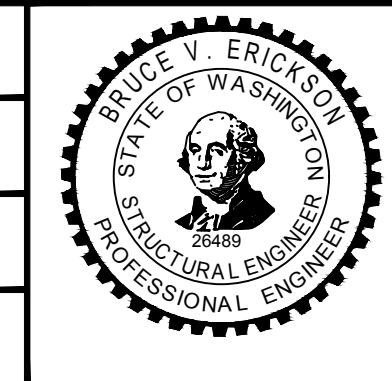


**ELEVATION**  
SCALE: NTS

10/27/20 | 2:10 PM | HAOYI  
 C:\CADD\BIP\W\H\WEST\DM\83012\L200-L85-SWD001.DWG

RUD	12/07/20	JS	BE	BE	REV UNDER DEVELOPMENT - SHORELINE TAR WALL
0	09/30/19				ISSUED FOR CONTRACT / CO 002
No.	DATE	DSN	CHK	APP	REVISION

DESIGNED BY:  
D. EVANGER  
 DRAWN BY:  
H. SHI  
 CHECKED BY:  
B. ERICKSON  
 APPROVED BY:  
B. ERICKSON

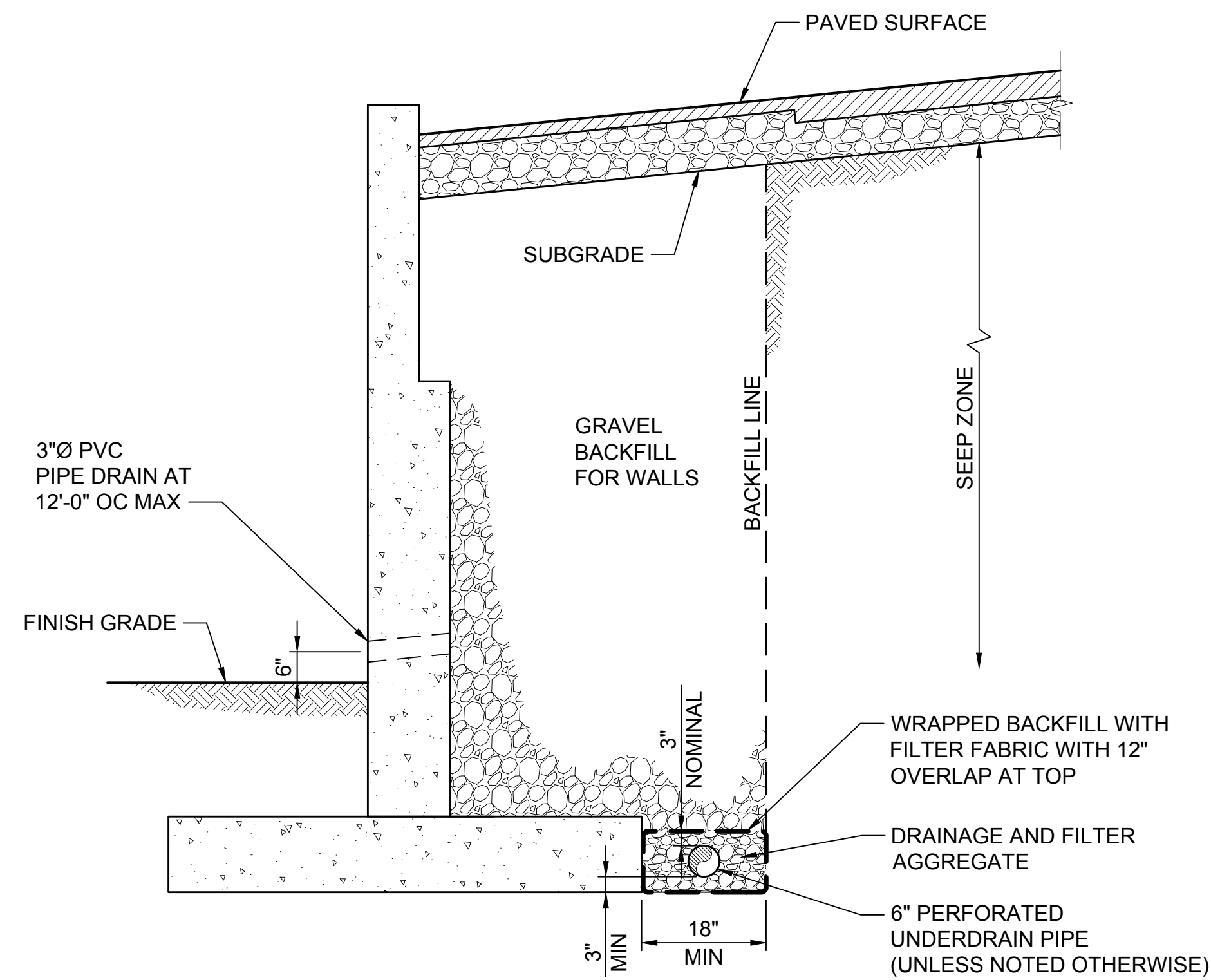


SCALE:  
NTS  
 FILENAME:  
L200-L85-SWD001  
 CONTRACT No.:  
RTA / CN 0079-15C  
 DATE:  
9/30/2019

**LYNNWOOD LINK EXTENSION**  
**CONTRACT L200**  
 NORTHGATE STATION TO NE 200TH STREET  
 STRUCTURES, WALLS  
 CAST-IN-PLACE CONCRETE WALLS  
 STANDARD DETAILS

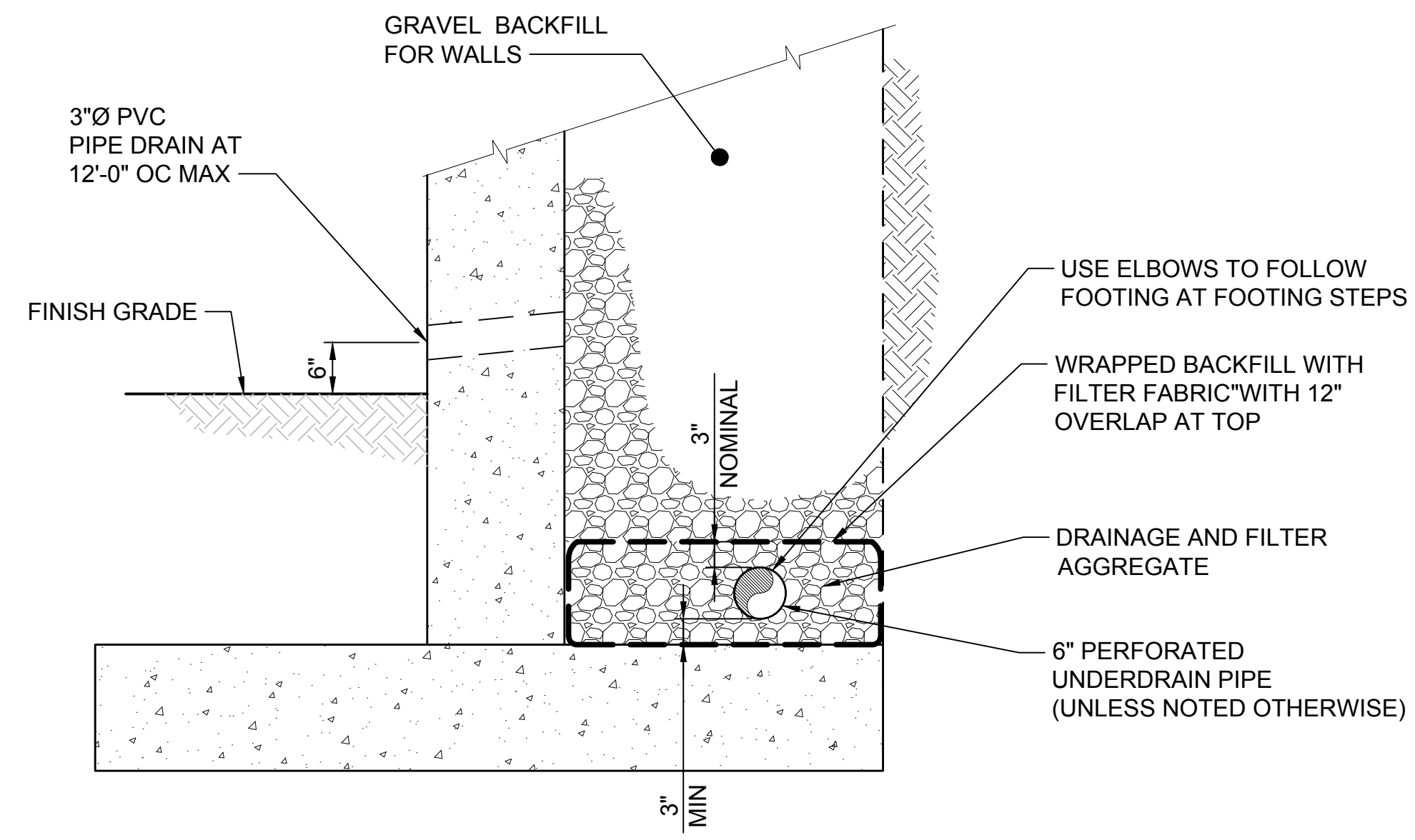
DRAWING No.:  
**L85-SWD001**  
 LOCATION ID:  
 SHEET No.:  
1704  
 REV:  
RUD

Xrefs:  
 XL200-GB-TB22x34  
 XLLE-STD-SWD001  
 GB-SEAL-BVC26489



**TYPICAL SECTION - CONDITION A**

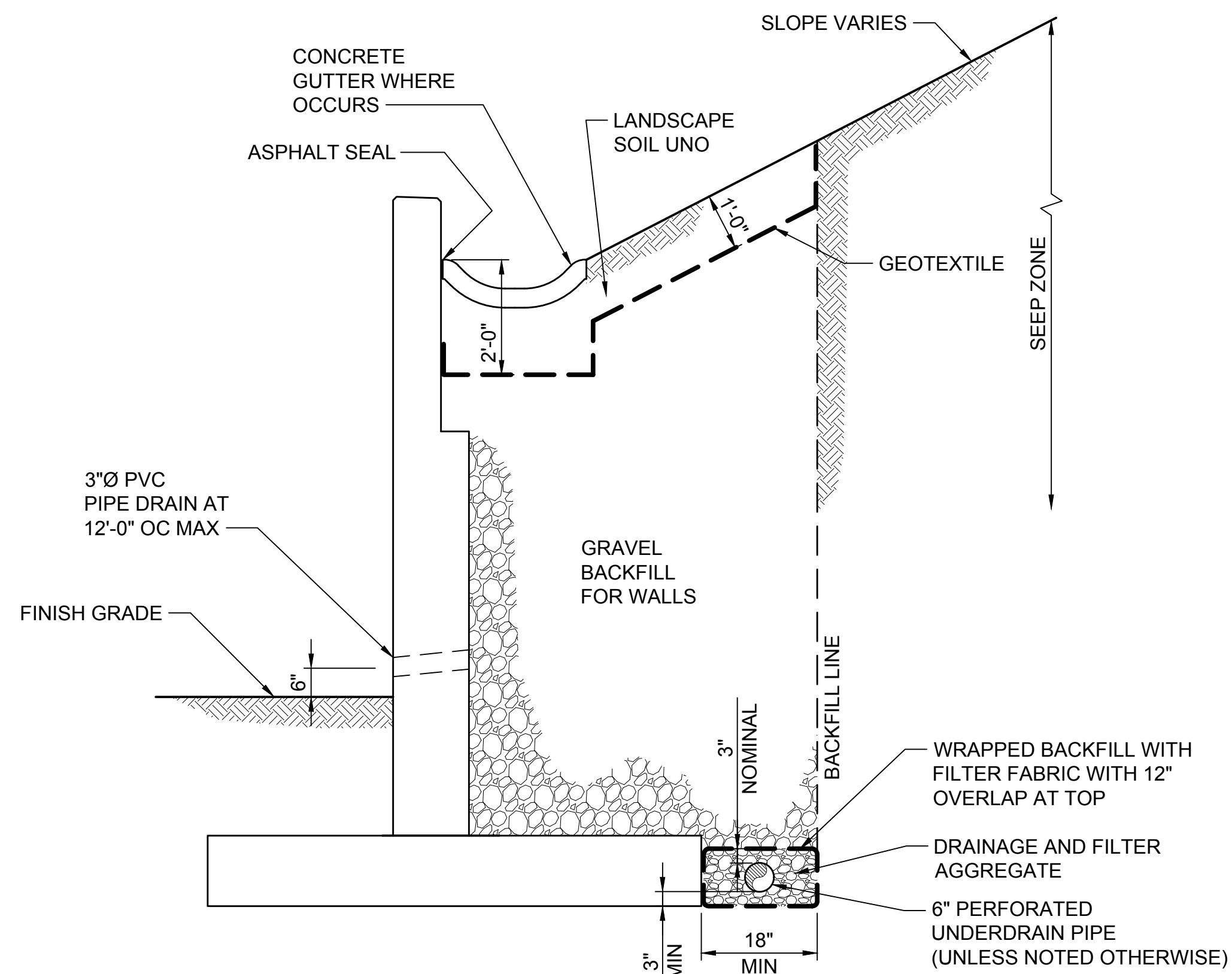
SCALE: NTS



**ALTERNATE UNDERDRAIN LOCATION SECTION**

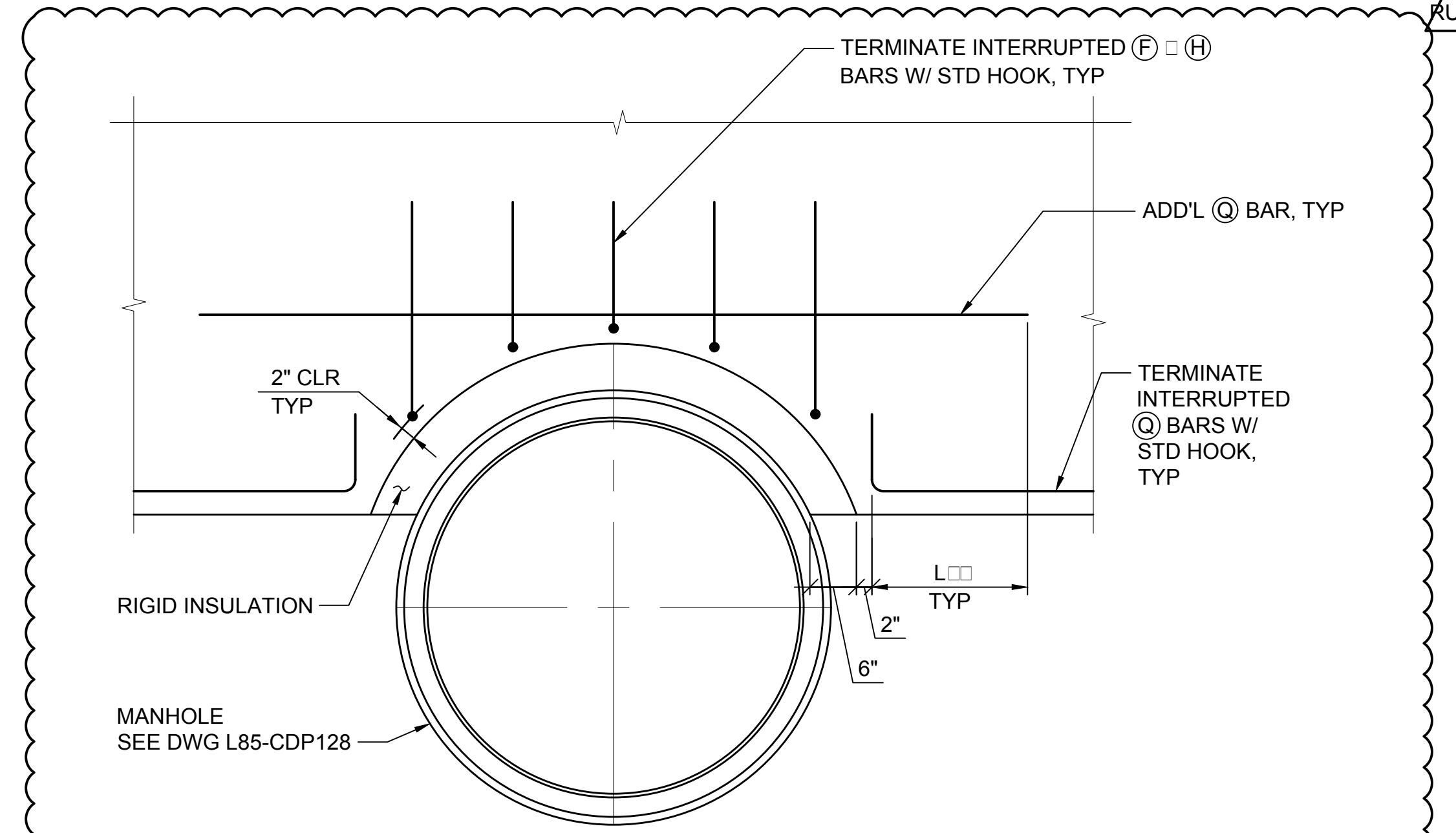
SCALE: NTS

- NOTES**
- SEE CONTRACT DRAWINGS AND SPECIFICATIONS FOR BACKFILL LIMITS AND GEOTEXTILE CLASS.



**TYPICAL SECTION - CONDITION B**

SCALE: NTS



- NOTES:**
- SEE DRAWING L85-SWD001 FOR BAR DESIGNATIONS.
  - ADDITIONAL REINFORCING SHALL MATCH SIZE PER DRAWING L85-SWD003.

**MANHOLE AT CIP FT**

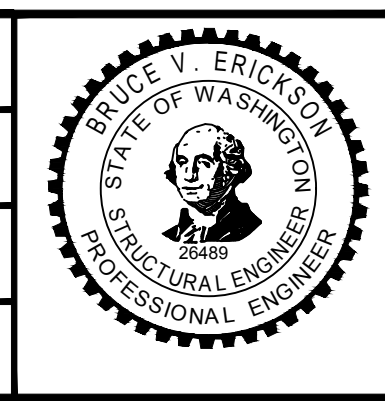
SCALE: 3/4" = 1'-0"

1  
 L85-SWD005  
 L85-SWP865

12/03/20 12:32 PM | HAOYI  
 C:\CADD\LIB\W\H\WEST\DM\863012\L200-L85-SWD005.DWG

No.	DATE	DSN	CHK	APP	REVISION
RUD	12/07/20	JS	BE	BE	REV UNDER DEVELOPMENT - SHORELINE TAR WALL
0	09/30/19				ISSUED FOR CONTRACT / CO 002

DESIGNED BY:  
D. EVANGER  
 DRAWN BY:  
H. SHI  
 CHECKED BY:  
B. ERICKSON  
 APPROVED BY:  
B. ERICKSON



**kpff**  
 SUBMITTED BY:  
S. BURCH

**HNTB Jacobs**  
 trusted design partners  
 DATE:  
9/30/2019

REVIEWED BY:  
F. CHIHAB  
 DATE:  
9/30/2019

LINE IS 1" AT FULL SCALE  
**SOUNDTRANSIT**  
 DATE:  
9/30/2019

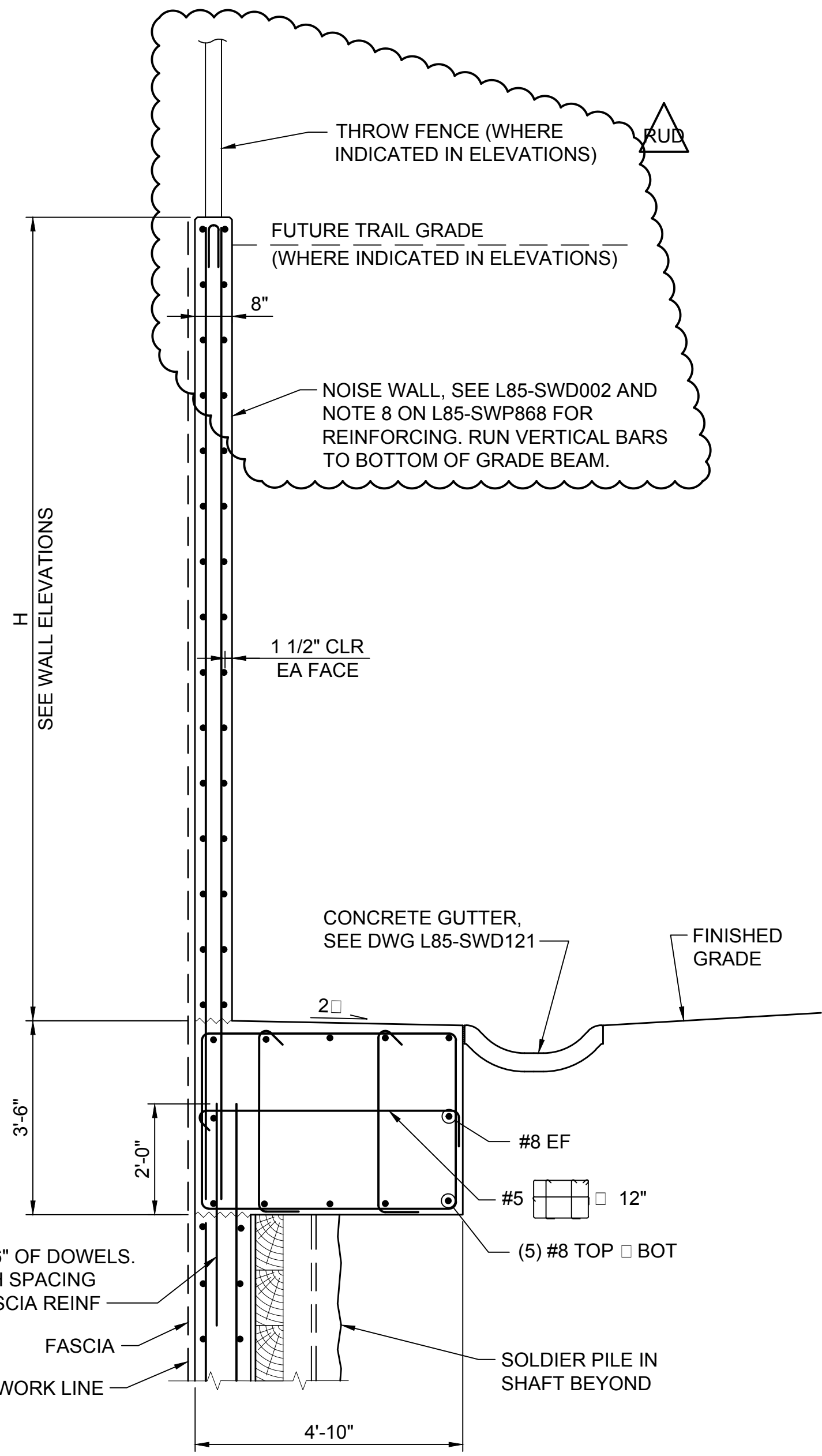
SCALE:  
NTS  
 FILENAME:  
L200-L85-SWD005  
 CONTRACT No.:  
RTA / CN 0079-15C  
 DATE:  
9/30/2019

**LYNNWOOD LINK EXTENSION  
 CONTRACT L200**  
 NORTHGATE STATION TO NE 200TH STREET  
 STRUCTURES, WALLS  
 CAST-IN-PLACE CONCRETE WALLS  
 STANDARD DETAILS

DRAWING No.:  
**L85-SWD005**  
 LOCATION ID:  
 SHEET No.:  
1708  
 REV:  
RUD

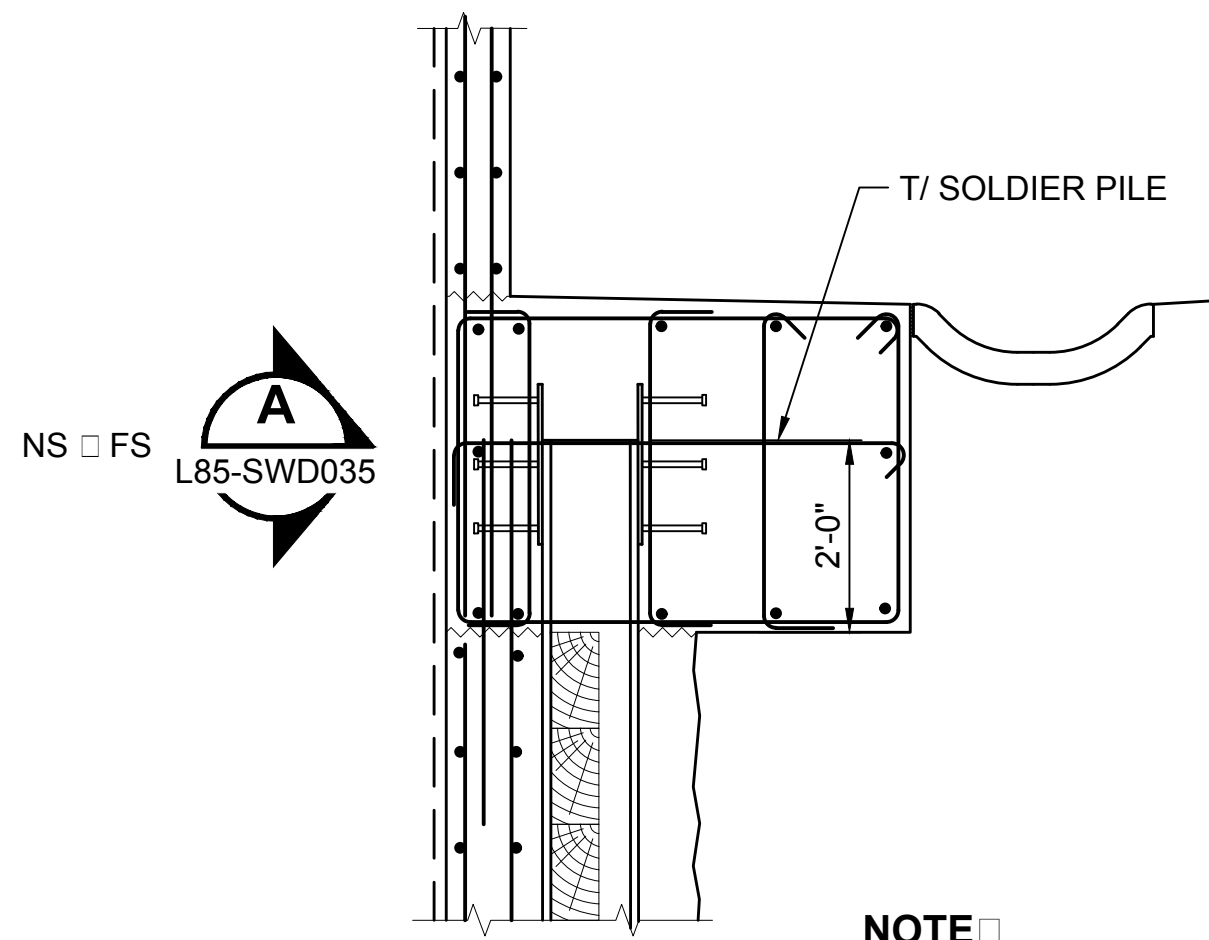


Xrefs:  
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 GB-SEAL-BW226489  
 XLLE-STD-SWD018



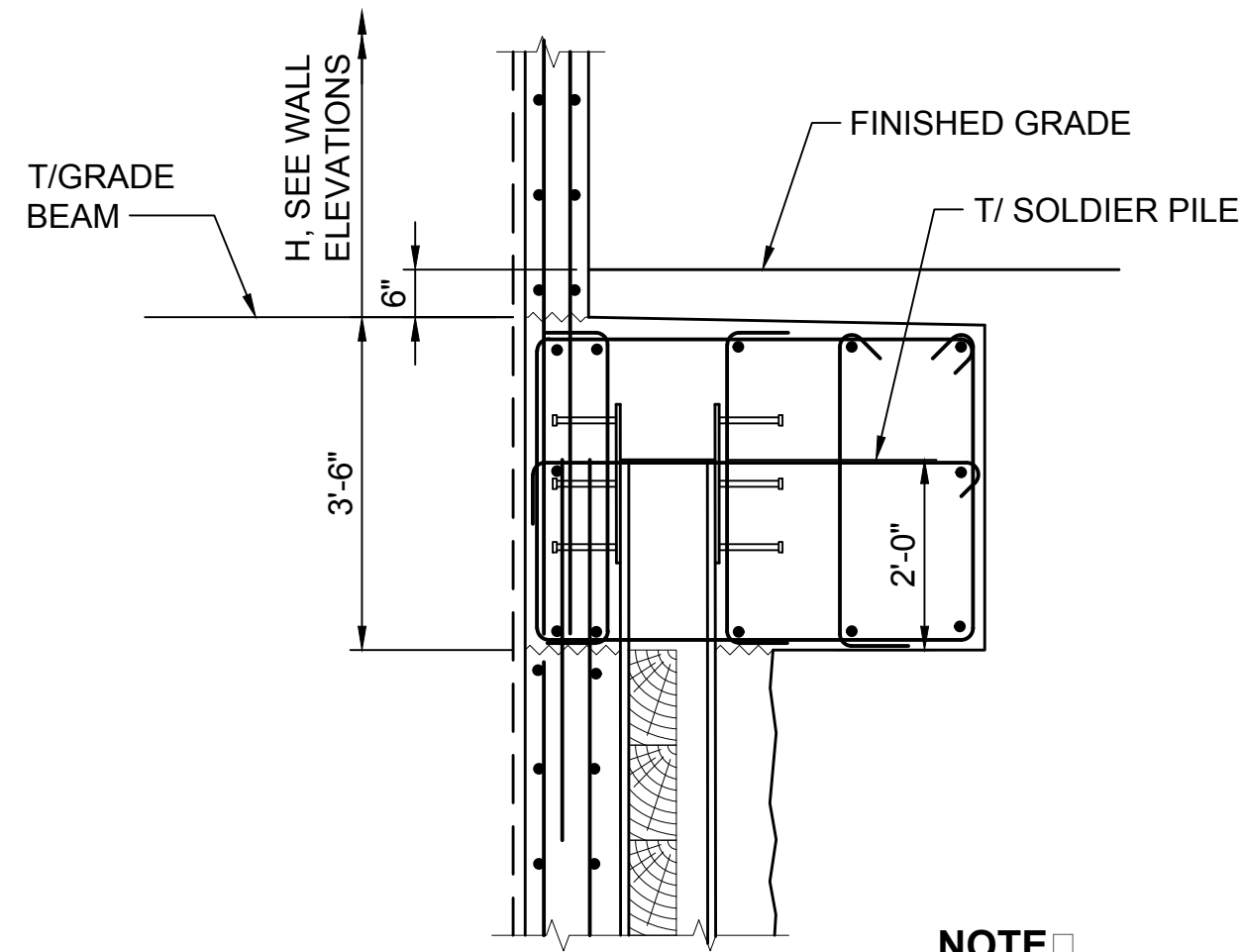
- NOTES:
- SEE DWG SERIES L85-CGP FOR GUTTER PLAN LOCATIONS.
  - SEE 2/L85-SWD034 FOR CONNECTION DETAILS AT SOLDIER PILE.

**SECTION - NOISE WALL AND GRADE BEAM AT SOLDIER PILE WALL**  
 SCALE: NTS  
 L85-SWD034  
 L85-SWX101



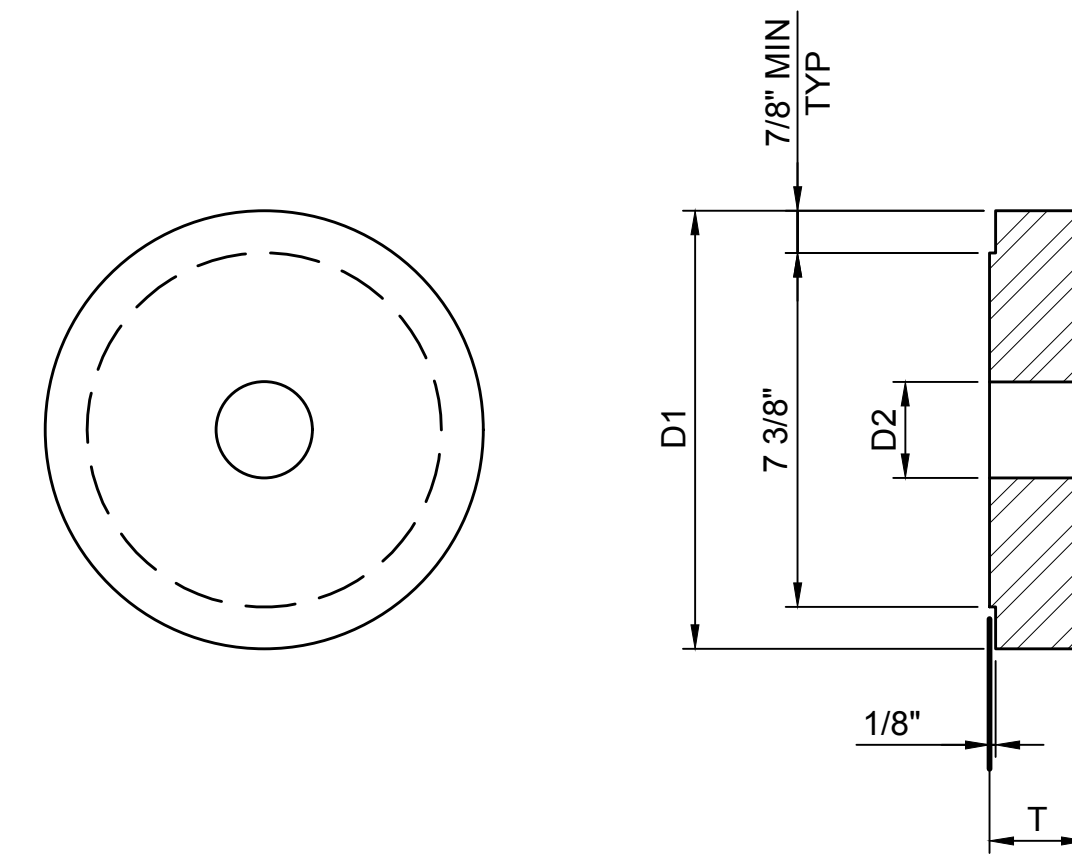
NOTE  
 SEE 1/L85-SWD034 FOR DETAILS NOT CALLED OUT HERE.

**CONNECTION DETAILS AT SOLDIER PILE**  
 SCALE: NTS  
 L85-SWD034



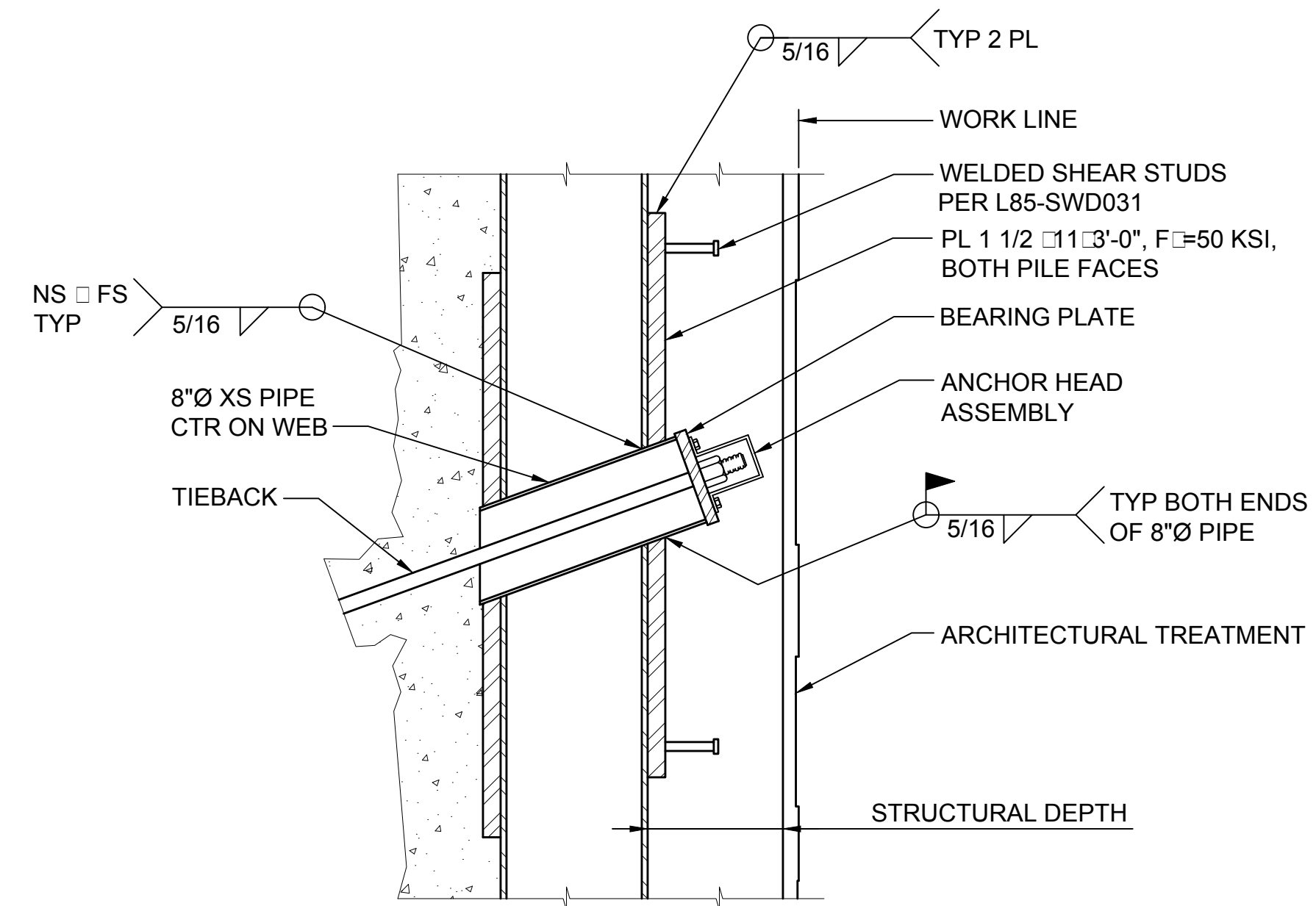
NOTE  
 SEE 2/L85-SWD034 FOR DETAILS NOT CALLED OUT HERE.

**CONNECTION DETAILS AT LOWERED GRADE BEAM**  
 SCALE: NTS  
 L85-SWD034  
 L85-SWP812  
 L85-SWP814  
 L85-SWP892  
 L85-SWP894



NOTE  
 BEARING PLATE SHALL BE DESIGNED BY THE CONTRACTOR AND SUBMITTED TO THE ENGINEER FOR APPROVAL.

**BEARING PLATE**  
 SCALE: NTS



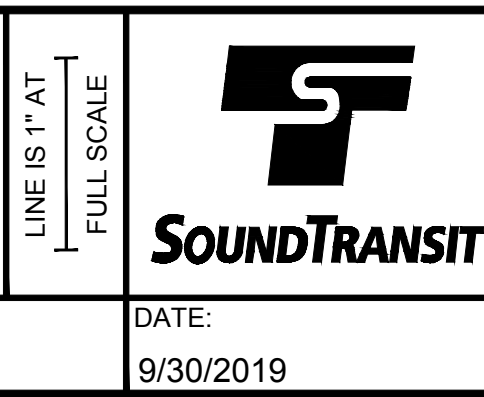
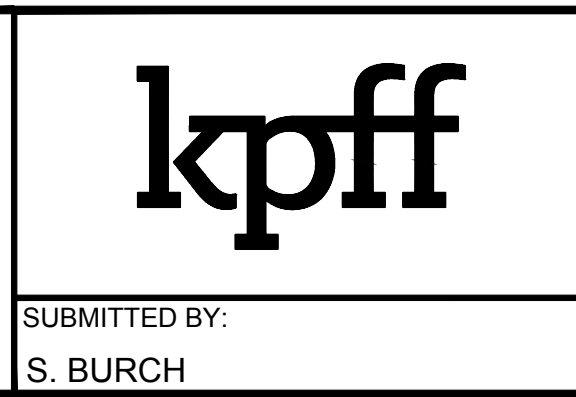
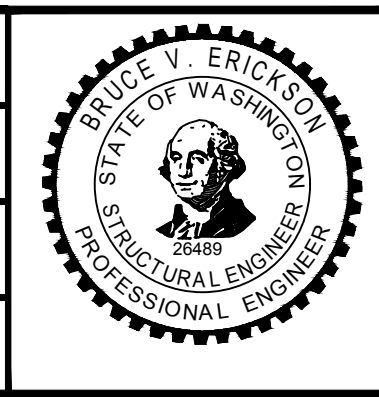
NOTE  
 SEE 2/L85-SWD034 FOR DETAILS NOT CALLED OUT HERE.

**SECTION - SOLDIER PILE WITH TIEBACK ANCHOR THROUGH WEB**  
 SCALE: NTS  
 L85-SWD034

12/04/20 10:09 AM HAOYIS  
 C:\CADD\BIP\W\H\WEST\DM\83012\L200-L85-SWD034.DWG

RUD	12/07/20	JS	BE	BE	REV UNDER DEVELOPMENT - SHORELINE TAR WALL
0	09/30/19				ISSUED FOR CONTRACT / CO 002
No.	DATE	DSN	CHK	APP	REVISION

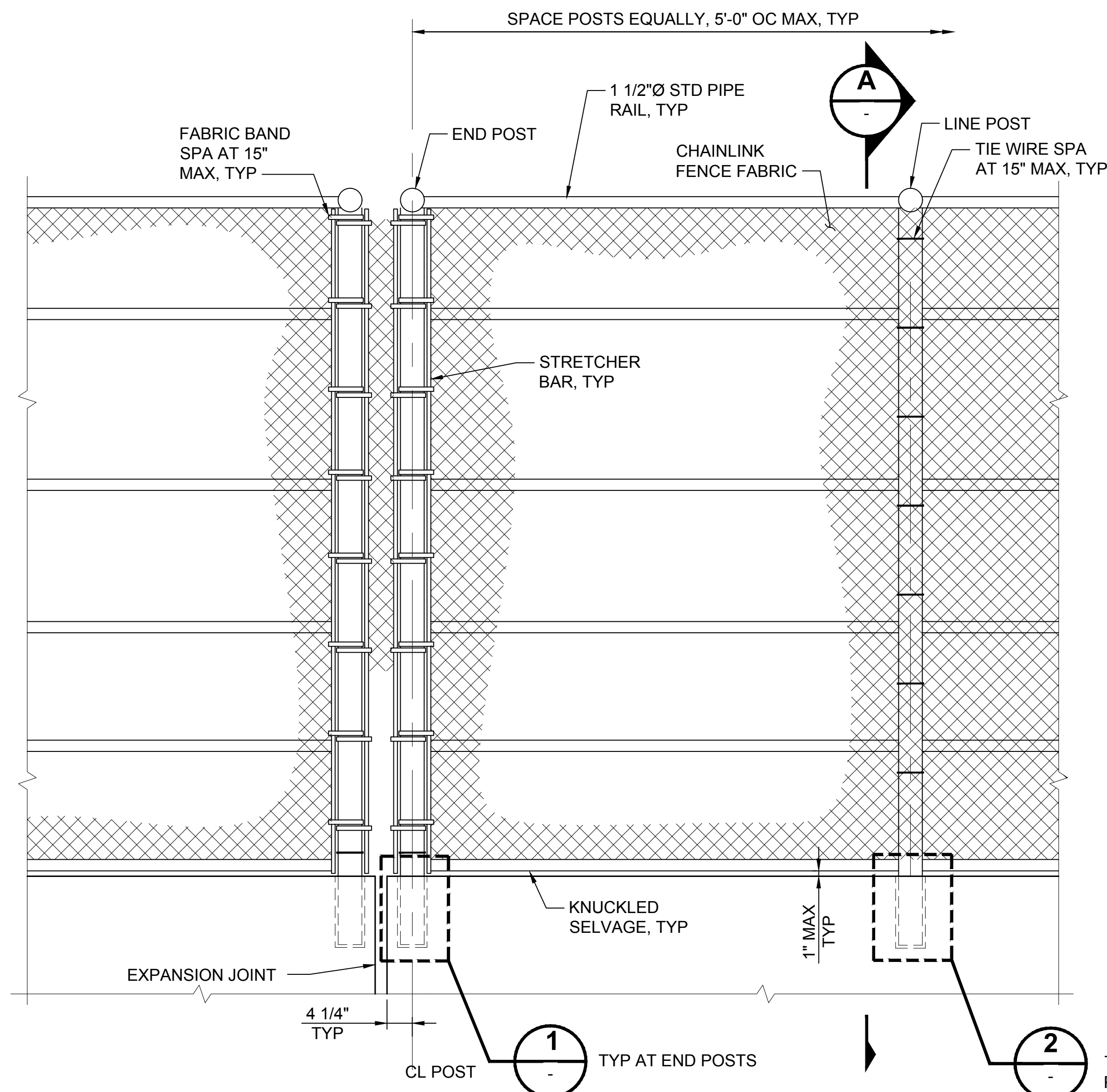
DESIGNED BY:	D. EVANGER
DRAWN BY:	H. SHI
CHECKED BY:	B. ERICKSON
APPROVED BY:	B. ERICKSON



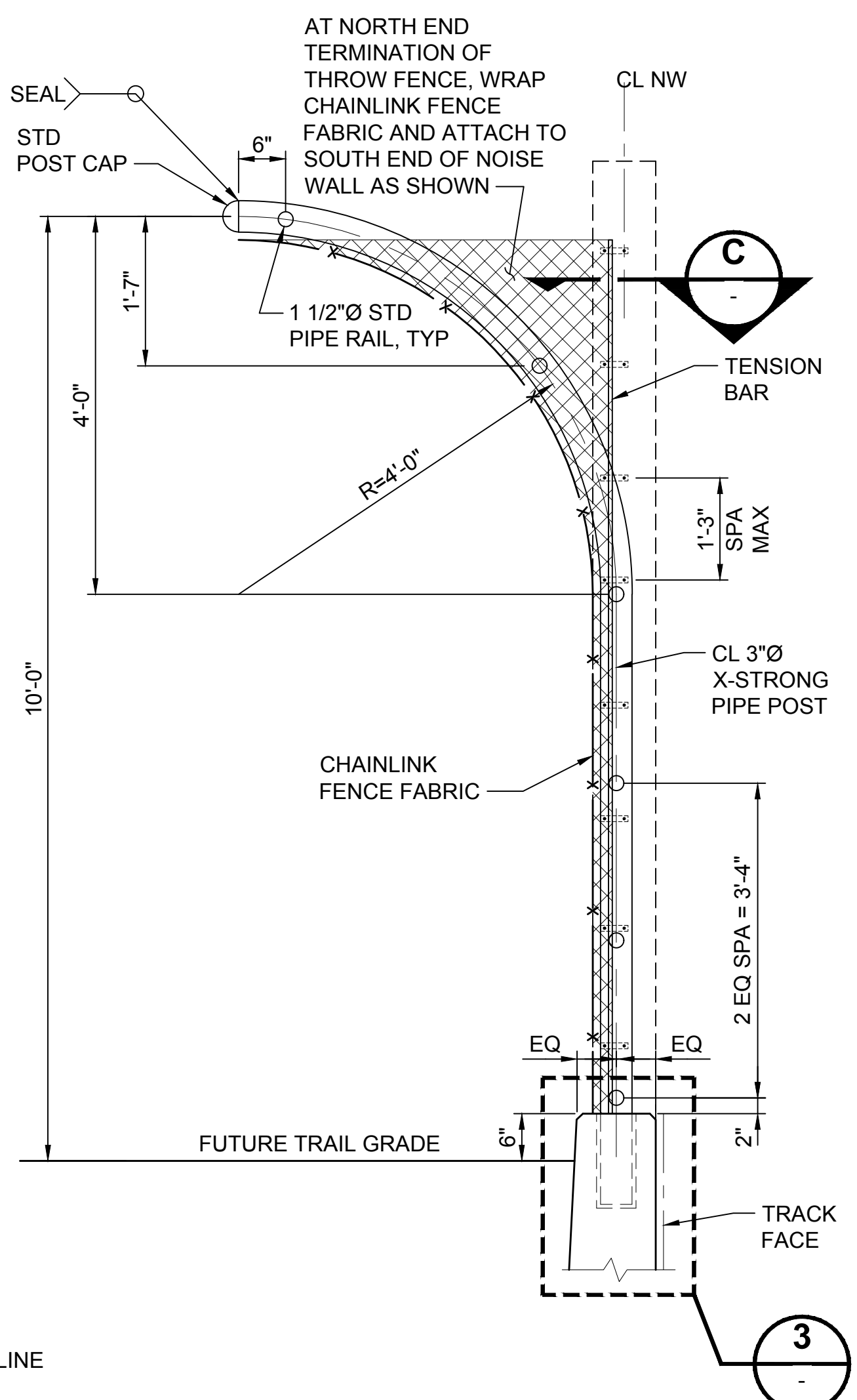
SCALE:	NTS
FILENAME:	L200-L85-SWD034
CONTRACT No.:	RTA / CN 0079-15C
DATE:	9/30/2019

<b>LYNNWOOD LINK EXTENSION CONTRACT L200</b>	
NORTHGATE STATION TO NE 200TH STREET	
STRUCTURES, WALLS SOLDIER PILE/TIEBACK WALLS STANDARD DETAILS	

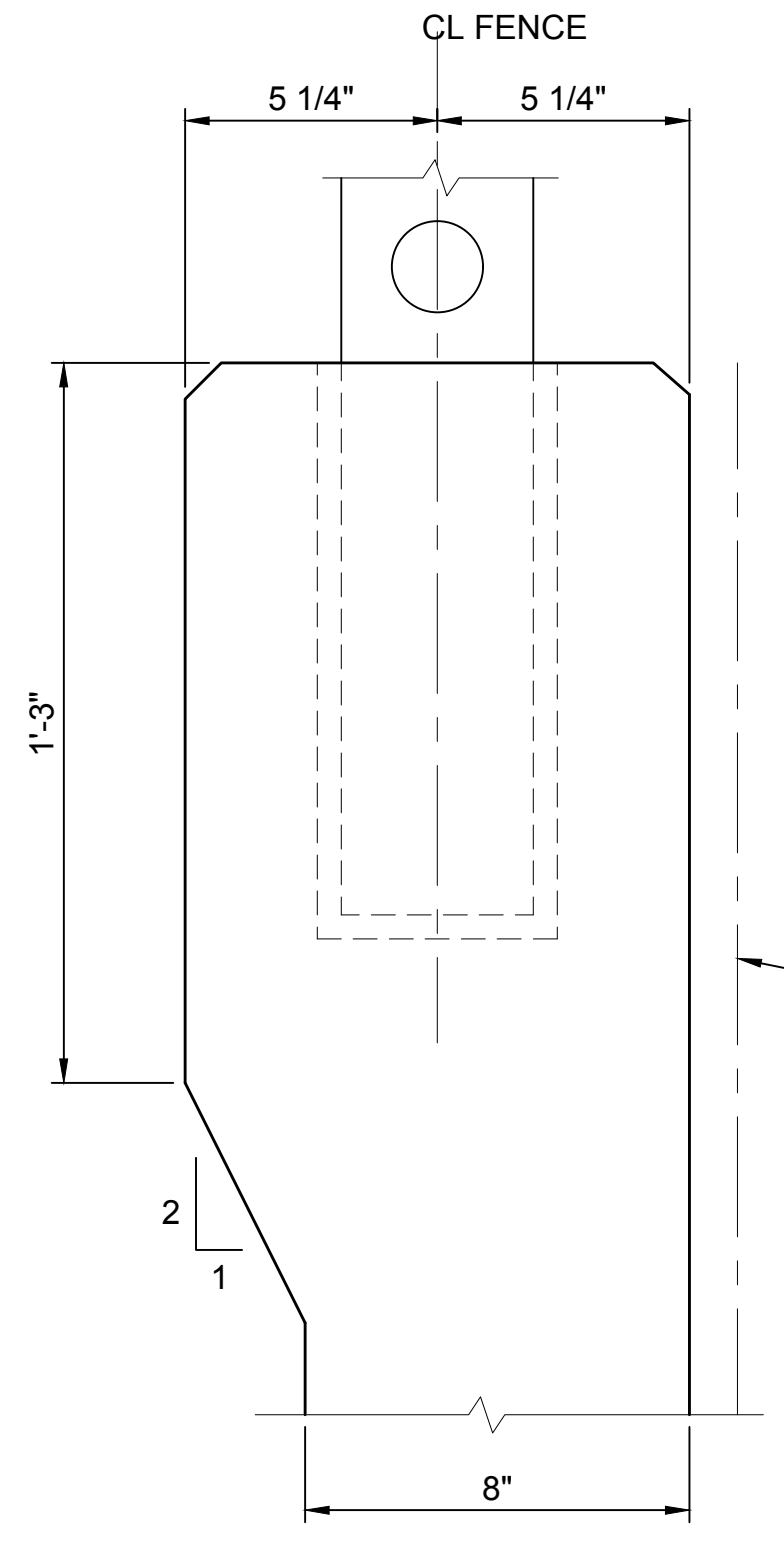
DRAWING No.:	L85-SWD034
LOCATION ID:	
SHEET No.:	1719
REV:	RUD



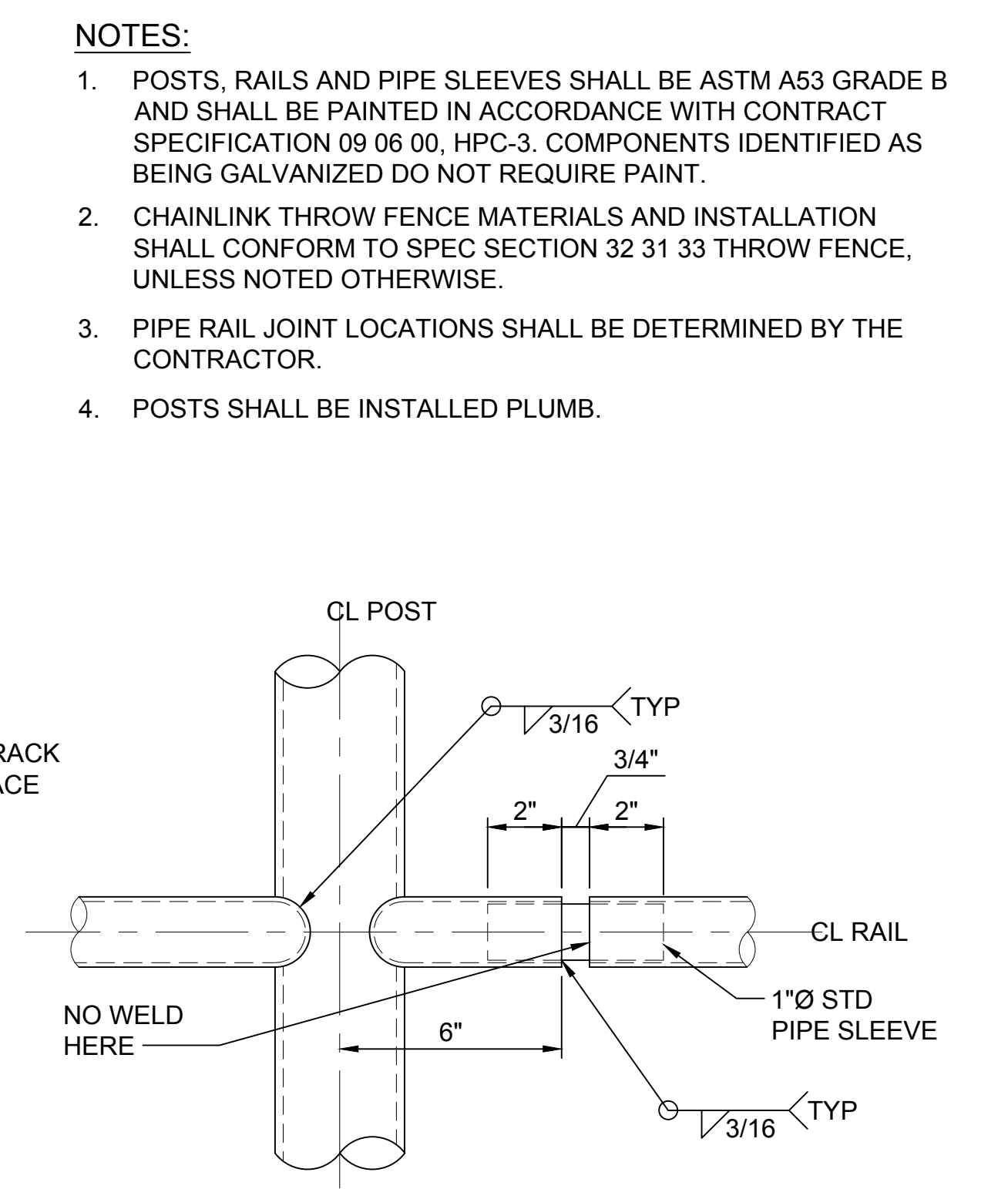
**INSIDE ELEVATION - THROW FENCE**  
 SCALE: 3/4" = 1'-0"



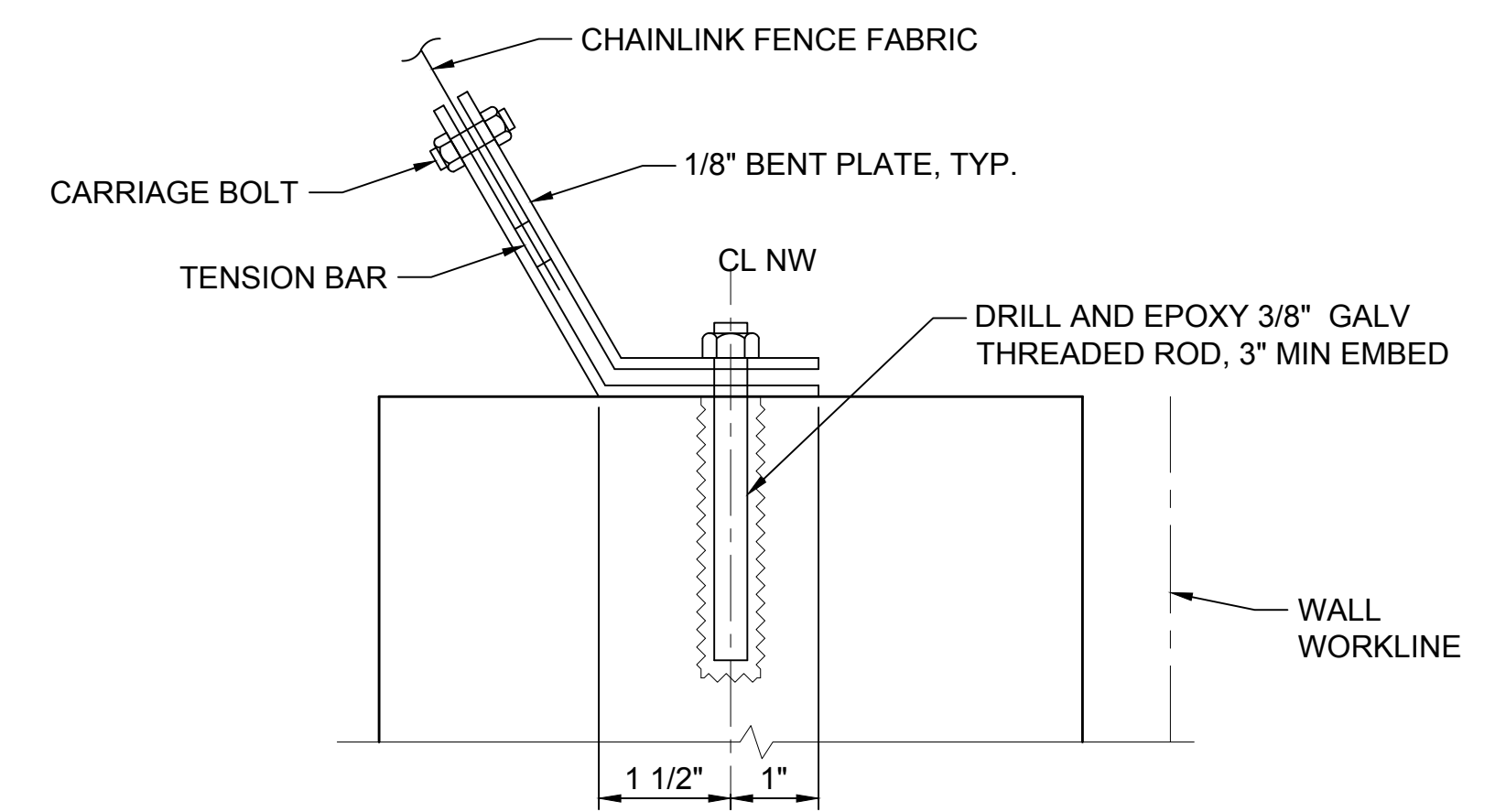
**SECTION A**  
 SCALE: 3/4" = 1'-0"  
 L85-SWD094  
 L85-SWP864  
 L85-SWP866  
 L85-SWP868



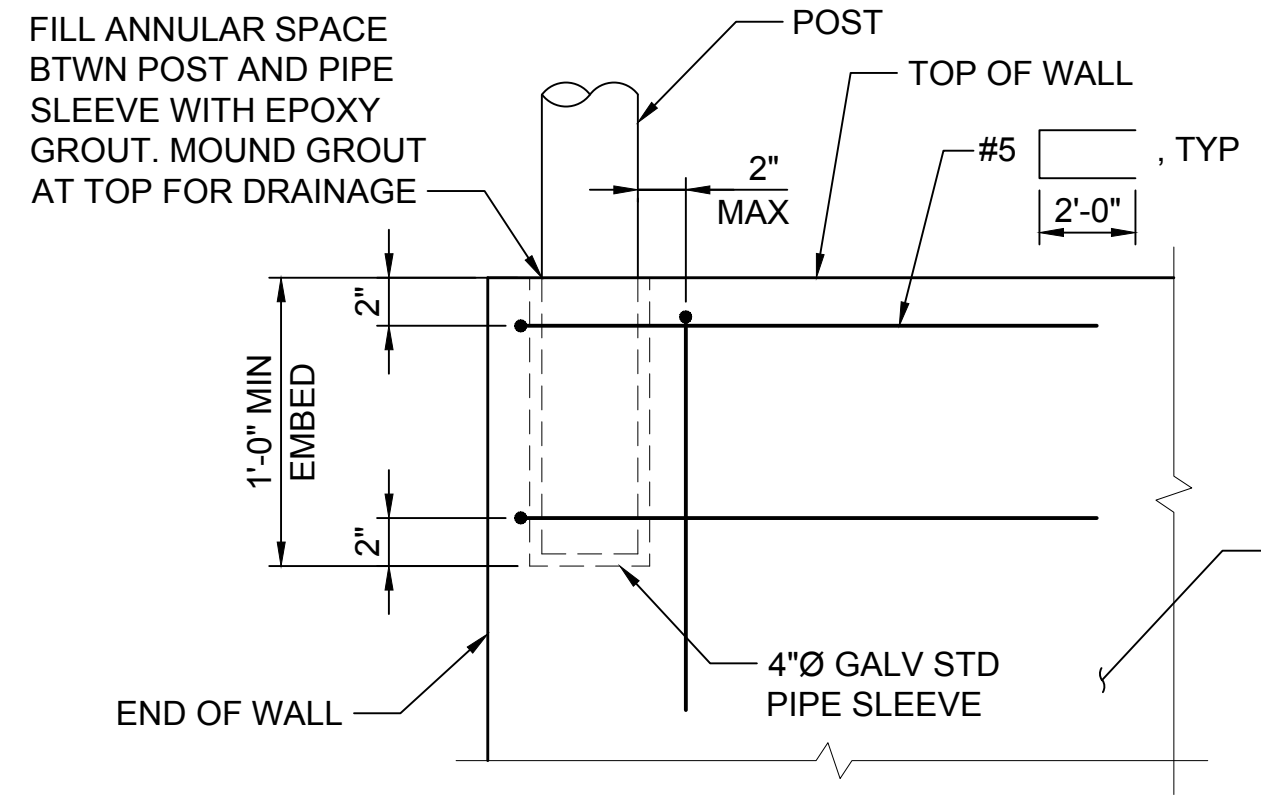
**DETAIL 3**  
 SCALE: 3" = 1'-0"  
 L85-SWD094



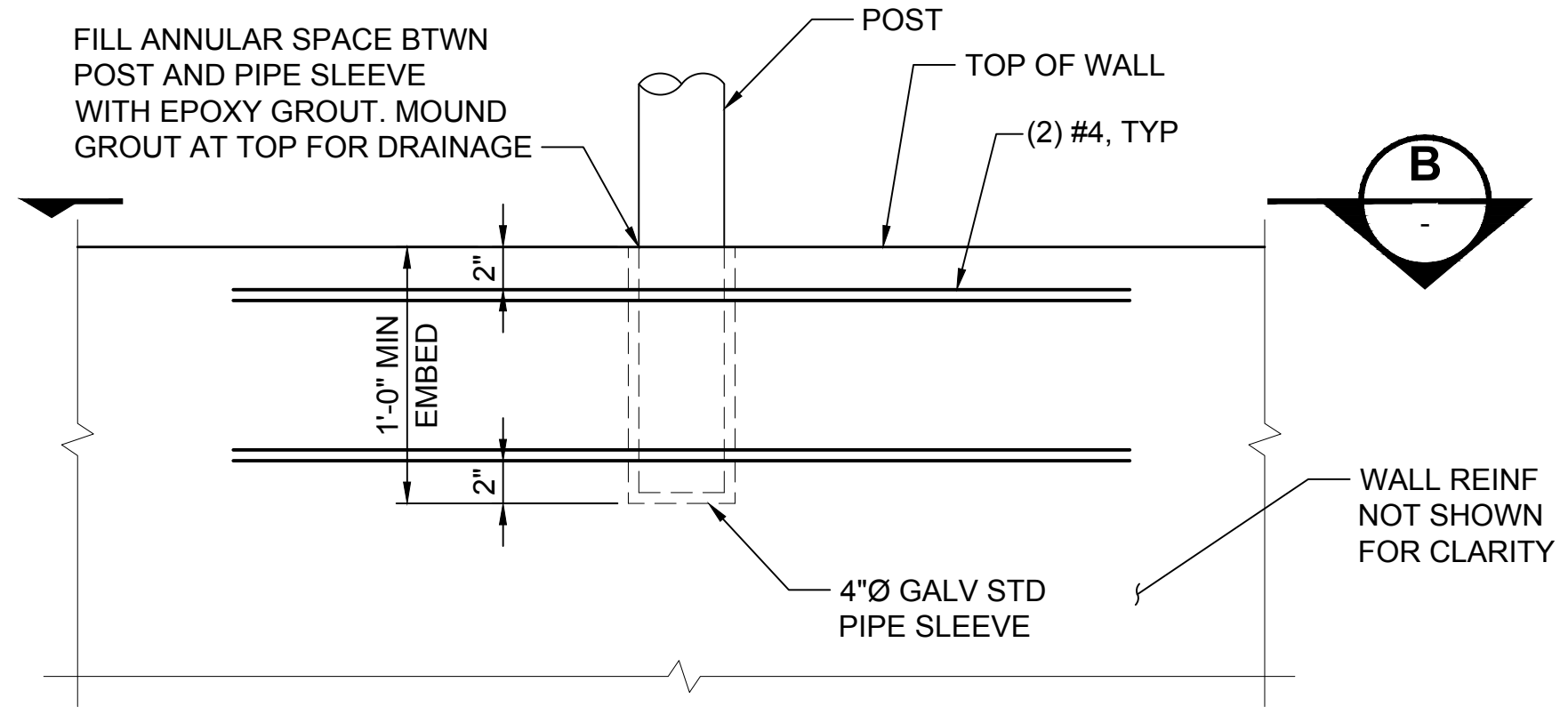
**PIPE RAIL JOINT DETAIL**  
 SCALE: 3" = 1'-0"



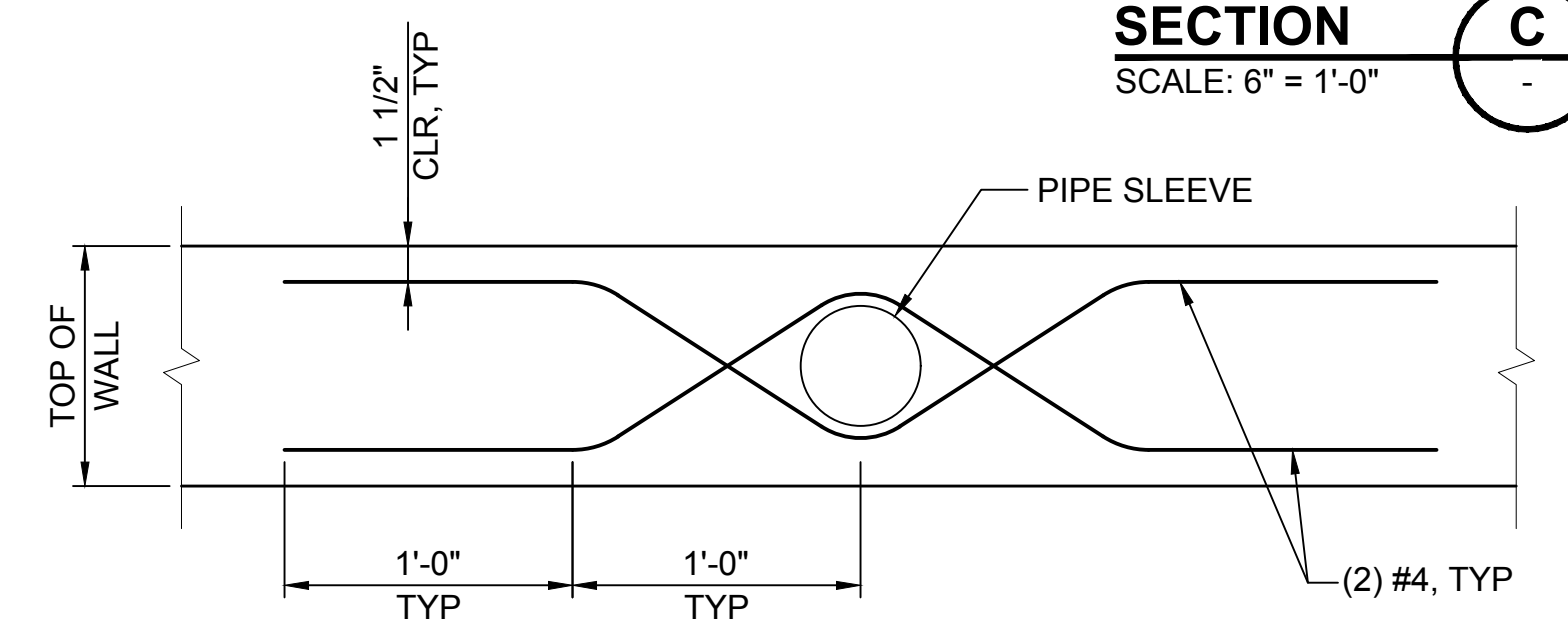
**SECTION C**  
 SCALE: 6" = 1'-0"



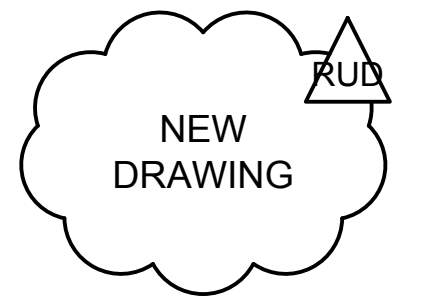
**DETAIL 1**  
 SCALE: 1 1/2" = 1'-0"



**DETAIL 2**  
 SCALE: 1 1/2" = 1'-0"



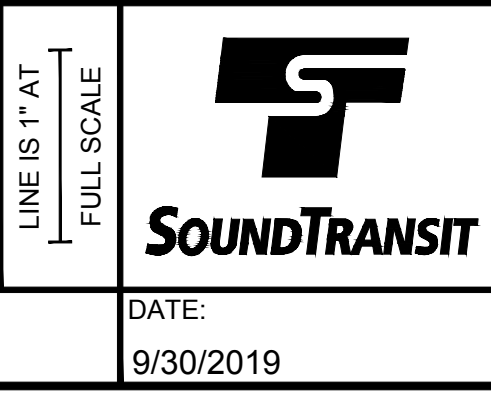
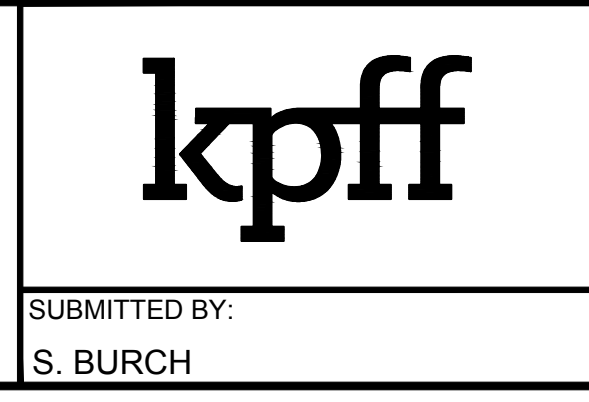
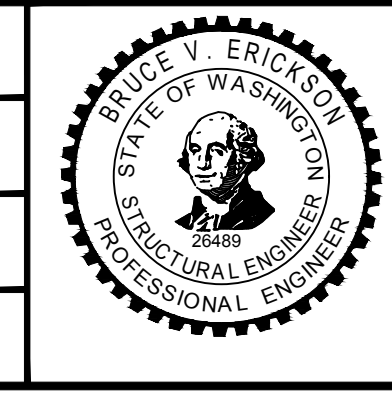
**SECTION B**  
 SCALE: 1 1/2" = 1'-0"



12/04/20 10:44 AM HAOYIS  
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No.	DATE	DSN	CHK	APP	REVISION
RUD	12/07/20				REV UNDER DEVELOPMENT - SHORELINE TAR WALL

DESIGNED BY:  
 J. STABLER  
 DRAWN BY:  
 H. SHI  
 CHECKED BY:  
 B. ERICKSON  
 APPROVED BY:  
 B. ERICKSON



SCALE:  
 AS NOTED  
 FILENAME:  
 L200-L85-SWD094  
 CONTRACT No.:  
 RTA / CN 0079-15C  
 DATE:  
 9/30/2019

**LYNNWOOD LINK EXTENSION  
 CONTRACT L200**  
 NORTHGATE STATION TO NE 200TH STREET  
 STRUCTURES, WALLS  
 MISCELLANEOUS  
 DETAILS

DRAWING No.:	<b>L85-SWD094</b>
LOCATION ID:	<b>N16</b>
SHEET No.:	<b>1743B</b>
REV:	<b>RUD</b>



**Exhibit B - Retaining Wall Cost Estimate**

<b>L200 RFP 012 SHORELINE TAR WALL rev. 1</b>													
<b>HCSS PRICING</b>													
Krebs Corporation 20-304-R2 Mike Weaver													
L200 Shoreline TAR Wall READ 2 Rev1													
<b>ESTIMATE SUMMARY - COSTS &amp; BID PRICES</b>													
Bid#	Client# Bid Description	Quantity	Unit	Manhours	Direct Labor	Perm Matl	Constr Matl	Equip-Ment	Sub-Contr	Direct Total	Indirect Charge	Total Cost	Total Cost Unit Price
1000	WALL 2628 PC NOISEWALL ON SHAFT	-324.00	LF	-366 1.13	-29,105	-190,203	-3,733	-11,231	-36,478	-270,750		-270,750	835.65
1010	EX / BF / SET PANELS (BP-28)	-4,716.00	SF	-361 0.08	-28,761	-190,203	-3,703	-11,099		-233,766			
1020	DRILLED SHAFTS	-230.00	LF						-23,727	-23,727			
1030	SHAFT REINFORCING	-6,150.00	LB	-4	-344		-30	-132	-8,035	-8,541			
1040	PIGMENTED SEALER	-4,716.00	SF						-4,716	-4,716			
2000	WALL 2628 B CIP WALL	3,825.00	SF	3,269 0.85	268,897	85,611	135,137	120,217	119,270	729,132		729,132	190.62
2010	EXCAVATION	707.00	CY	37 0.05	4,992		35,376	5,080		45,448			
2020	BACKFILL	626.00	CY	134 0.21	10,397	37,289	1,964	5,463		55,313			
2030	CIP CONC WALL FTG	129.00	CY	488 3.79	39,664	16,617	7,285	16,261	16,724	96,552			
2040	CIP CONC WALL	231.00	CY	2,389 11.21	213,644	31,704	90,512	93,413	102,546	531,818			
3000	WALL 2634 NOISE WALL ON SP WALL	119.00	SF	88 0.74	6,902	453	1,677	1,735	10,309	21,076		21,076	177.11
4000	21% SELF PERFORM FEE	1.00	LS				81,135			81,135		81,135	81,134.97
4100	27% SUBCONTRACT FEE	1.00	LS				25,137			25,137		25,137	25,137.12
<b>Totals:</b>				2,990	246,694	-104,138	239,352	110,720	93,100	585,729		585,729 *	[ 585,730 ]

\* Following comparison of this Internal Cost Estimate with the cost estimate prepared by Sound Transit's L200 Contractor - SKH Joint Venture, a final price of \$590,000 was negotiated and agreed to by the City and SKH Joint Venture.



## **EXHIBIT C - DESIGNATED REPRESENTATIVES**

Pursuant to Section I of the Agreement, the following individuals are the Parties' Designated Representatives:

### **SOUND TRANSIT**

Barbara Hinkle

Project Manager – Lynnwood Link Ext.

Phone: (206) 370 - 5698

Email: [barbara.hinkle@soundtransit.org](mailto:barbara.hinkle@soundtransit.org)

### **CITY OF SHORELINE**

Tricia Juhnke, P.E.

City Engineer

Phone: (206) 801-2471

Email: [tjuhnke@shorelinewa.gov](mailto:tjuhnke@shorelinewa.gov)