



## Memorandum

**DATE:** March 31, 2011

**TO:** Shoreline Planning Commission

**FROM:** Joseph W. Tovar, FAICP, Planning and Development Services Director  
Paul Cohen, Senior Planner *PC*

**RE:** April 7, 2011 Study Session on Town Center Subarea Plan and Development Code

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### Introduction

This study session's purpose is to present and discuss the staff's proposal for the Town Center Subarea Plan and Development Code.

Project Location: Between Linden Ave. N. and Stone Ave. N. and between N 188<sup>th</sup> and N 170<sup>th</sup> Streets.

Recent Development: The area of the Subarea Plan is approximately 88 acres. Recent development includes City Hall, Interurban Trail, Top Foods, Aurora Avenue, Shoreline Fire Station, Gateway Plaza, Key Bank, Carson Building, Sky Nursery, and Walgreens. Redevelopment permits have been applied for Aurora Rents and Shorewood High School.

Comprehensive Plan Land Use Designations: The majority of the area is designated Community Business with relatively minor amounts of Mixed Use along the west edge of Stone Avenue and the south half of Firlands triangle and Regional Business on the north half of Firlands triangle. City-wide, the Community Business designation was changed to Regional Business.

The following 2007 policies establish the framework for development of the land use, capital facility and programmatic aspects of the Town Center Subarea Plan.

FW-1 Articulate a community vision for the town center as an early step in the development of detailed provisions for the subarea.

FW-2 Establish a study area boundary to provide context for evaluating the opportunities and potential impacts from future development of commercial and mixed uses along Aurora Ave. N.

FW-3 Engage Shoreline residents and businesses in detailed design processes for a ) a park site on both sides of the Interurban Trail and b) Midvale Ave N.

FW-4 Design roadway, transit and pedestrian facilities consistent with the City's preferred "Flexible alternative" for Aurora Avenue between N. 165<sup>th</sup> St. and N. 205<sup>th</sup> St.

FW-5 Prepare a program of civic directional or 'way finding ' signage and evaluate refinements to city sign regulations to reflect the emerging function and visual character of Aurora Avenue.

Zoning: The majority of the area is zoned Mixed Use Zone with relatively minor amounts of R-48 and Office zones on the Town Center periphery. City-wide, the Mixed Use Zone and its development standards were converted from Regional Business zoning in 2010.

### **Background**

A town center has been a perennial topic for the City since before incorporating in 1995. In 1996 the City Council identified this area as a commercial and civic center in their Visioning Map. In 1998 the community identified the general area around N.175th and Aurora Avenue as the "Heart of Shoreline". In 2003 the Planning Commission recommended a report supporting a plan for Central Shoreline. In 2007 the City approved 13 Strategic Points to guide for developments and improvements in Town Center until a plan was adopted. Also in 2007 the Council adopted Phase 1 of the Town Center Plan, which replaced the 13 strategic points with 5 Town Center framework goals for the Comprehensive Plan.

Between 2008 and 2010 the City held 1 design workshop, 3 city-wide meetings, 2 surveys (245 respondents each), a walking tour, 4 meetings with the adjoining neighborhoods, 2 meetings with Stone and Linden Avenue neighbors, a speaker series on related planning topics, and 2 UW planning classes to study Town Center. In addition, the City met with representatives of Forward Shoreline, Chamber of Commerce, Economic Development Committee, car dealerships, Top Foods, Fred Meyer, Aurora Rents, Ronald Methodist Church, School District, Highland Ice Arena, and Interurban Building.

Public noticing included the City website, Currents, signage in the town center, Facebook (96 friends), Shoreline Area News, email list of approximately 300 stakeholders, and all property owners. The early drafts of the policy plan and code have been available to the public since December 1, 2010. The City received numerous written communications which can be viewed on-line at <http://shorelinewa.gov/index.aspx?page=180>.

City staff from Planning, Economic Development, Public Works, and Parks departments met regularly over the past several years to coordinate ongoing city projects such as private development, Aurora Avenue, park design, banners, and draft Town Center policies and code.

### **Discussion**

#### **Subarea Plan**

The first proposal is to amend the Comprehensive Plan with the Town Center Subarea Plan. Then last draft of this plan was presented to the PC December 2, 2010. The subarea plan is a policy plan that includes the Planning Commission vision statement, principles of sustainability, goals and policies.

To distinguish the current draft from the December draft, you will note that it is labeled the "3-29-11 DRAFT". Rather than show the changes from the December draft in revision format, we have simply used yellow highlighting where the proposed Town Center Policies are substantially new or different. These changes appear in the Policies listed below. Following each is an explanation of the nature and reason for these changes:

TC-2 Staff proposes to use the term "green infrastructure" to describe these public projects.

TC-4 We thought it appropriate to make this policy more active and directive by emphasizing "reducing parking requirements" and "pursuing an aggressive program of property tax exemptions" as ways to decrease costs and thereby increase housing.

TC-16 We added language to clarify that protecting the residential neighborhood east of Aurora also included the medium density zoning behind Midvale, while the method of such protections for neighborhoods west of Aurora would take the form of limitations on vehicular access and noise/signage impacts.

TC-18 We added policy language referring to the upcoming U.S. Green Building Code and the desirability of a program to recognize exemplary projects.

TC-20 We added policy language to explain that sustainability for adjacent residential neighborhoods might take the form of green street investments or energy conservation programs for homes in those areas.

TC-21 We added the active verb "reduce" for parking requirements and "in recognition of" transit opportunities, rather than the prior language "explore opportunities to reduce". We decided to ratchet up this commitment, in part because the "Rapid Ride" bus rapid transit service is now only two years away. We have also found a good webpage that describes what the service on Aurora will look like. The following link describes the same Rapid Rise service that starts this year in South King County. <http://metro.kingcounty.gov/tops/bus/RapidRide/>

TC-23 Some of the public comment at a prior Town Center Open House called for a "Town Center" walking program in conjunction with the resources of the Shoreline Historical Museum. We thought adding policy language to that effect would be appropriate. While two historically significant buildings are just outside the Town Center (the Ronald School Building and the Masonic Temple, at opposite ends of Linden Avenue N.) they are within an easy walk of the red brick road at Town Center. These three prominent artifacts of Shoreline History could be the major focal points of such a walk.

TC-25 We have re-worded this policy statement to emphasize that our permit process would include not just review of design (pursuant to standards) but also the environmental review that will have been done with the Planned Action EIS. We thought this updated policy language better reflects some of the input from our Economic Development Manager that we want to be able to communicate to potential developers/investors that the City's permit process in Town Center will be as predictable and easy as possible.

### Development Code

The second proposal is to amend the Development Code with Town Center code to implement the subarea plan. This proposal is mostly same proposal presented to the Planning Commission in December, 2011.

Overall Organization: The organization of the development standards is to first address the purpose, land uses and dimensional standards. The zoning has been divided into 4 districts to further distinguish their land uses, and development dimensions, and design standards.

- TC-1 Aurora Southwest – Same as TC-2 except allows vehicle sales.
- TC-2 Aurora – Emphasizes commercial development with some residential uses and pedestrian activity internal to the blocks that front primarily along Boulevards type streets.
- TC-3 Firlands / Midvale – Emphasizes residential development with some commercial development and pedestrian activity primarily along Storefront Streets.
- TC-4 Stone Avenue – Exclusively medium density residential while allowing single family development.
- Transition Overlay – This overlay adds building height restrictions and landscape screening between residential and Town Center.

The design standards are articulated into 5 connecting elements that work together in order to build Town Center into a continuous, well-functioned, and attractive district.

- Neighborhood Protection – Visual and traffic impact mitigation and access to amenities in the Town Center.
- Street Frontage – Dimensional and design standards for streets, sidewalks, and way-finding signs that are appropriate to different modes of transportation (including pedestrian) and appropriate to the adjoining land uses.
- Site Design – Site design and building placement as it complements the streetscape and connects activity from the public street/sidewalk internal to sites.
- Building Design – Contributes to a more accessible, distinctive, and attractive neighborhood.
- Signage – Major contribution and complement to the built environment and the visibility for business. Signs can bring activity and people which contribute to the walkability and interest in Town Center.

### Highlights of the Town Center Code

- Form-based code that emphasizes function and appearance of development especially as it enhances the public and semi-public areas.
- Design review approval process required prior to construction permits.
- Designation of a park in Town Center.
- Emphasis on surrounding neighborhood protections.
- Increased east/west pedestrian connections including a signal at Aurora Avenue and N 180<sup>th</sup> St.
- Supports vehicle sales only in TC-1.
- Aurora Ave., 175<sup>th</sup>, and 185<sup>th</sup> Streets to be more car-oriented, Midvale and Firlands to be more pedestrian-oriented, and Stone and Linden Avenues to be more residential-oriented.

- Greater site and building design standards in exchange for full development potential.
- No incentives for height increases including density limits.
- Clear thresholds to relieve small sites from some site requirements.
- Allows some requirements to be combined, but not diminished, such as walkways and Through-connections and public gathering and open space.
- Require commercial spaces but not uses.
- Require green infrastructure and anticipates green building codes in the near future.

### MUZ / Town Center Comparison

- Same land uses as previous RB zoning / Adds list of prohibited uses.
- Maximum 150 Dwelling Units per Acre / Density limited by building envelope.
- Maximum height 65 feet / Maximum height 70 feet (TC-1, TC-2, TC-3).
- Minimum height 35 feet / No minimum height.
- Building heights stepped up from 45 to 65 feet after 200 horizontal feet of bordering R-4 through R-12 / heights stepped from 35 to 70 feet after 110 horizontal feet in TC-4 and Transition Overlay.
- Setbacks from non- residential zones / No setback requirement for non-residential zones.
- Parking area screening and public gathering places / Same plus full and comprehensive street, site, building and signage, neighborhood protection standards.
- Both require design review.
- Potentially slight increase in residential density in Town Center.

### State Notice and SEPA Compliance

The Washington State Department of Commerce was provided the required 60-day notice on November 18, 2010 and March 29, 2011.

The City of Shoreline issued a SEPA Threshold Determination of Significance (DS) on November 22, 2010, and distributed an Environmental Impact Statement (EIS) Scoping Letter to the public and surrounding jurisdictions, agencies, and tribes. During this 30 day scoping period, the City received one comment letter, from the City of Mountlake Terrace. The City is currently preparing a Draft EIS, which it anticipates being ready for distribution by the end of April 2011. The Draft EIS has a 30 day comment period; at the conclusion of this 30 day period, the City will have a maximum of 60 days to issue a Final EIS.

The EIS will evaluate the Town Center Subarea Plan and Town Center Code as a "Planned Action", per RCW 43.21C.031. The purpose of a Planned Action is to shift environmental review from the time a permit application is made to an earlier phase in the development process. As such, future development proposals within the Town Center boundaries that are consistent with the development assumptions of the subarea would be exempt from SEPA.

Future projects would still be subject to all applicable codes and regulations, and larger products would require a traffic study and administrative design review.

**Comprehensive Plan Amendment Criteria**

The amendment is consistent with the Growth Management Act and not inconsistent with the Countywide Planning Policies, and the other provisions of the Comprehensive Plan and City policies; or

The amendment addresses changing circumstances, changing community values, incorporates a sub area plan consistent with the Comprehensive Plan vision or corrects information contained in the Comprehensive Plan; or

The amendment will benefit the community as a whole, will not adversely affect community facilities, the public health, safety or general welfare. (Ord. 238 Ch. III § 7(f), 2000).

**Development Code Amendment Criteria**

The amendment is in accordance with the Comprehensive Plan; and

The amendment will not adversely affect the public health, safety or general welfare; and

The amendment is not contrary to the best interest of the citizens and property owners of the City of Shoreline. (Ord. 238 Ch. III § 7(g), 2000).

**Next Steps**

Staff has scheduled May 5<sup>th</sup> and May 19<sup>th</sup> for public hearings on the proposal and a meeting June 2 for final recommendations. If the Commission wishes, the May dates can be used for another study session and a public hearing or keep both for 2 public hearings.

**Recommendation**

Staff recommends that the Planning Commission select May 5<sup>th</sup> and May 19<sup>th</sup> for public hearings. If all of May 19<sup>th</sup> is not needed then the Commission could use the remaining time for deliberations.

**Attachments**

Attachment 1 – 3-29-11 Draft Subarea Plan

Attachment 2 – 3-31-11 Draft Town Center Development Code - Chapter 20.92

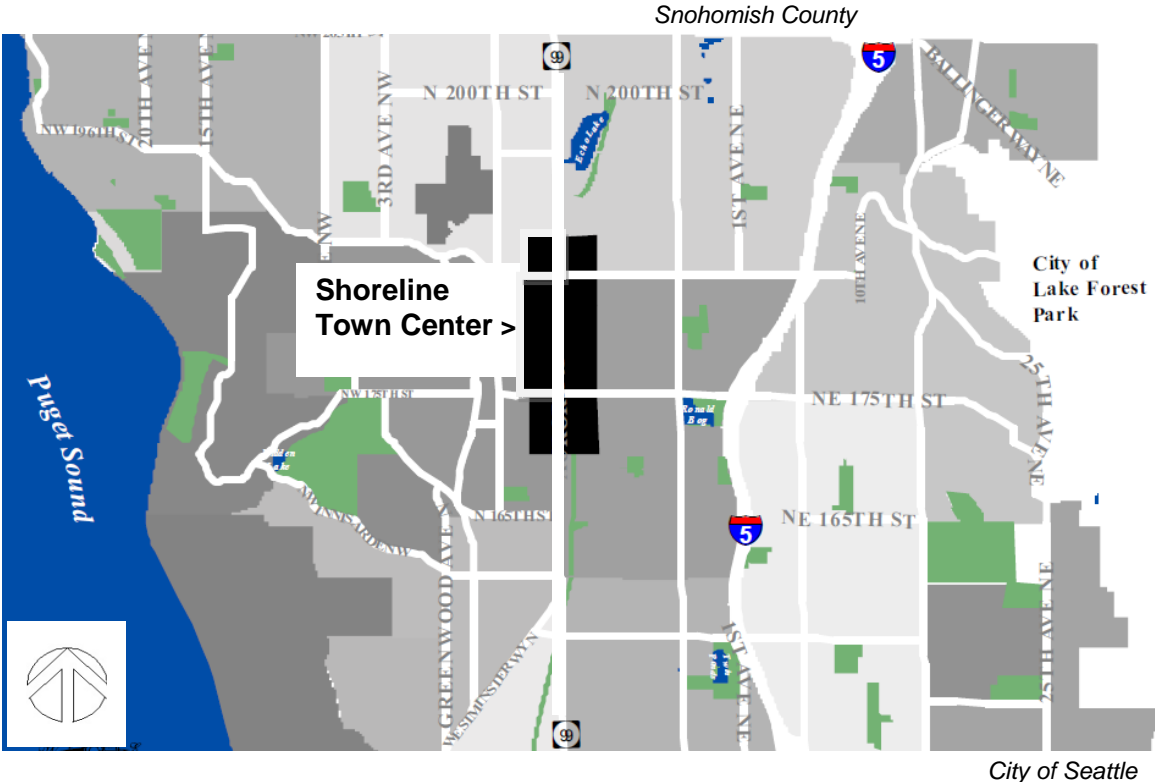


# Shoreline Town Center Subarea Plan

**DRAFT**  
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## Introduction

Located on the middle mile of the City’s three mile long Aurora corridor, Town Center is the geographic center of the City of Shoreline. It is at the crossroads of three of the City’s most heavily traveled roads, N. 175<sup>th</sup> St, N. 185<sup>th</sup> St., and Aurora/SR 99, and serves as the civic and symbolic center of the community. See Fig. 1. Early in the life of the new City of Shoreline, a citizen survey identified this area as the “Heart of Shoreline.”



**Fig.1** *Town Center is the Heart of Shoreline*

Shoreline’s settlement began in the early 20<sup>th</sup> century in this area around Judge Ronald’s original homestead and the Ronald schoolhouse. In the early 1900’s, the North Trunk (red brick) Road and Interurban electric railway traversed this area, linking it to Seattle and Everett. The “Ronald Station” was located in the vicinity of the proposed Park at Town Center.

Growing dramatically after World War II, Shoreline became an auto-oriented suburb characterized by large areas of relatively low residential density, which lacked urban amenities and services such as parks and sidewalks. During the post-war decades, the Aurora/SR 99 corridor developed as a strip commercial highway, with a tremendous diversity of businesses. While these businesses largely met local and regional needs, the highway itself became congested, chaotic, unattractive and unsafe.

Several of the civic facilities typically found in traditional downtowns began to locate in and around the Town Center area in the 1960's. These include the Shorewood High School, the Shoreline Fire Department Headquarters, and the Ronald Sewer District Office and Yard. Commercial and apartment uses also began to locate in this area, including grocery, drug store and other retail stores and personal services. These still co-exist with businesses serving a larger market area, such as auto dealerships.

The emergence of regional shopping malls at Alderwood and Northgate in the 1970's began to erode Shoreline's primary market for certain retail goods and services. With the City's incorporation in 1995, additional civic pieces of an emerging Town Center came into being. The Interurban Trail through Town Center was completed in 2005 and the new City Hall opened in 2009. In 2011 Aurora Avenue North through Town Center was rebuilt as a Boulevard, design work began on a new park at Town Center and construction began on a new Shorewood High School, with buildings located immediately adjacent to the Town Center.

In 2009, the City adopted a city-wide Vision Statement which articulated the community's preferred future for the year 2030. The Vision integrated many of the policy objectives of the City's adopted strategies for Economic Development, Housing, and Environmental Sustainability. The Vision identifies Town Center as a focal point for much of the City's future growth accommodation, and many of the framework goals provide a broad outline for much of the content of the Town Center Subarea Plan.

Achieving the City's Vision and the objectives of the Town Center Subarea Plan will be influenced by regional market factors, individual investment decisions, and state and regional growth management policies. High capacity transit service will arrive on Aurora by 2013 in the form of bus rapid transit service, while regional light rail service is scheduled for 2023, linking Shoreline to the broader region.

The growth management development strategy for the Central Puget Sound region, Vision 2040, forecasts adding 1.7 million people and 1.4 million jobs with only a negligible increase in the size of the region's urban growth area. See Fig. 2. Combined with state climate change targets to reduce greenhouse gas emissions and vehicle miles traveled, there will be strong market and regional public policy pressures on close-in cities such as Shoreline to accommodate growth.



Shoreline’s ability to accommodate these pressures while maintaining the community’s reputation as one of America’s best places to live, will be a major challenge. Implementation of a clearly articulated Town Center Subarea Plan will be one important strategy to help Shoreline meet that challenge.



**Fig. 2** Shoreline’s place within the Vision 2040 Urban Growth Area

**Town Center Vision Statement**

*Shoreline Town Center in 2030 is the vibrant cultural and civic heart of the City with a rich mix of housing and shopping options, thriving businesses, and public spaces for gatherings and events. People of diverse cultures, ages, and incomes enjoy living, working and interacting in this safe, healthy, and walkable urban place.*

*Once a crossroads on the Interurban electric railway that connected Seattle and Everett, Shoreline’s Town Center has evolved into a signature part of the City. The Center stands out as a unique and inviting regional destination while gracefully fitting in with its surrounding landscape and neighborhoods. Connections to neighborhoods and the region are convenient and accessible through a system of paths, roads and public transit. Citizens, business owners and city officials are justifiably proud of the many years of effort to create a special and livable place that exemplifies the best of Shoreline past, present and future.*

*Town Center is anchored along N. 175<sup>th</sup> St. by the City Hall complex, Shorewood High School, the Shoreline Fire Department Headquarters, and the Ronald Sewer Offices and Yard. The linear park at Town Center between Aurora Boulevard and Midvale Avenue North provides a green thread through the center of the area. City Hall serves not only is the seat of government, but also provides an active venue for many other civic functions. The north end of Town Center includes the revitalized historic five-point interchange at Firlands.*

*Town Center is a physically and visually attractive, inviting and interesting place where form and function come together to promote a thriving environment for residents, businesses, and visitors. Notable features include a number of green open spaces both large and intimate, enclosed plazas, storefronts opening onto parks and wide sidewalks, underground and rear parking, numerous ground-floor and corner retail options within mixed-use buildings, and internal streets within large blocks and other pathways that provide safe, walkable connections throughout the Center area both east and west and north and south.*

*Building heights range from one to three stories within transition areas adjacent to single-family residential areas such as Linden and Stone avenues, up to six stories in mixed-use buildings along sections of Aurora Boulevard, while buildings in the Midvale and Firlands areas are generally four to five-story mixed-use structures. Building materials, facades, designs, landscaped setbacks as well as public art and green infrastructure features represent a wide variety of styles and functions while maintaining a harmonious look and feel.*

*The City of Shoreline has long been committed to the realization of the three E's of sustainability -- environmental quality, economic vitality and social equity -- and Town Center has successfully integrated these values to achieve sustainable development.*



**Fig. 3** *Principles of Sustainable Development*

Environmental Quality

*While respecting elements of its historic character, Town Center has become a model of environmentally sound building and development practices. The buildings themselves are state-of-the-art energy efficient and sustainable structures with zero carbon impacts. Town Center's tree canopy and native vegetation are all part of a strategic system for capturing and treating stormwater on site and protecting and enhancing overall environmental quality. Major transit stops along the mature Aurora Boulevard provide quick and convenient connections to major centers elsewhere in the region. Civic spaces and parks have been designed for daily use and special events.*

Economic Vitality

*Town Center attracts a robust mix of office, service and retail development. The boulevard boasts an exciting choice of shops, restaurants, entertainment, and nightlife. The Center is a model of green industry and economic sustainability that generates the financial resources that help support excellent city services, with the highest health and living standards. As a result, Town Center's success helps to make Shoreline one of the most fiscally sound and efficiently run cities on the West Coast.*

Social Equity:

*Town Center offers a broad range of job opportunities and housing choices that attract a diversity of household types, ages and incomes. Attention to design allows the public gathering places to be accessible to all. People feel safe here day and night. Festivals, exhibits and performances attract people of all ages and cultural backgrounds.*

Summary:

*Town Center is thoughtfully planned and built, yet all the choices feel organic and natural as if each feature and building is meant to be here. Town Center is a place people want to be in Shoreline in 2030 and is positioned to continue to grow gracefully and sustainably for decades.*

**Town Center Goals**

**Goal TC-1** Create a Town Center that embodies the sustainability values of environmental quality, economic vitality and social equity.

**Goal TC-2** Create a Town Center that is complete, compact and connected to its neighborhoods and the region.

**Goal TC-3** Create a “sense of place” in Town Center that provides a focal point for Shoreline’s civic life and community-wide identity.

**Goal TC-4** Create an economically and culturally thriving Town Center through the coordinated efforts of the City, the School District, business organizations, community non-profits, and neighborhood associations.

**Town Center Policies**

**Policy TC-1** Create a safe, attractive, and walkable Town Center that links mixed use, mid-rise buildings, a broad range of housing choices, major civic amenities, public gathering places and bus rapid transit service.



**Fig.4** Mid-rise, mixed use buildings provide pedestrian scale and access at street level

**Policy TC-2** Publicize innovative “green infrastructure” including City Hall, Shorewood High School, and Aurora boulevard as models for private projects in Town Center.



**Fig. 5** The LEED GOLD City Hall, LEED SILVER Shorewood High School, and low-impact drainage facilities in the Aurora project set a high bar for sustainability in new projects

**Policy TC-3** Promote a blend of civic, commercial and residential uses in Town Center.

**Policy TC-4** Increase the variety of housing choices in Town Center and increase opportunities for moderate cost housing. Reduce new housing construction costs and incentivize affordable housing in Town Center by reducing parking requirements and pursuing an aggressive program of Property Tax Exemptions.

**Policy TC-5** Encourage additional retail, service, grocery, and restaurant uses to serve people who live or work in Town Center or within walking distance of it.

**Policy TC-6** Leverage federal, state and other investments and market Town Center as a high value location for private investment and business starts.



Fig. 6 The Interurban Trail, Aurora Project and Bus Rapid Transit service

**Policy TC-7** Give clear visual indication of Town Center’s boundaries with gateway treatments, such as signs and landscaping.



Fig. 7 Examples of town center entry signs

**Policy TC-8** Create a hierarchy of Boulevard, Storefront, and Greenlink streets to serve different mobility and access roles within Town Center.

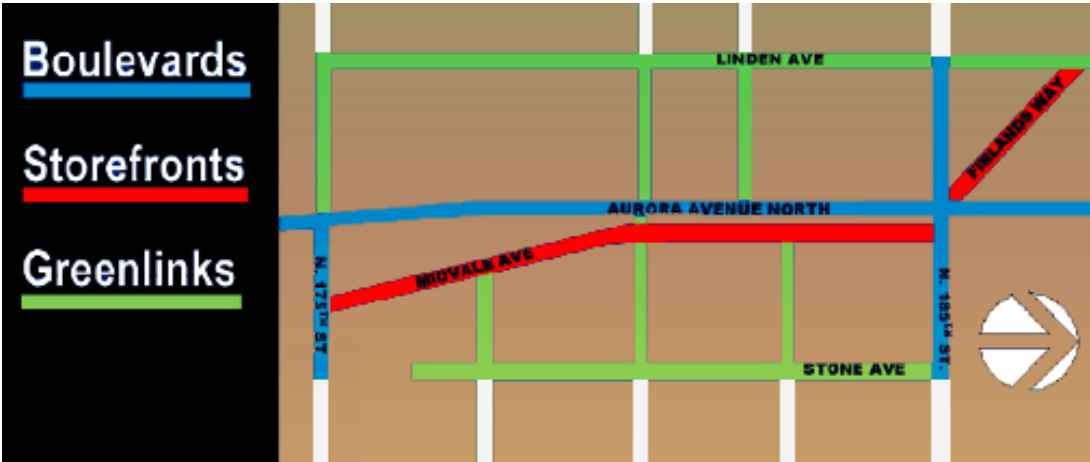


Fig. 8 A hierarchy of boulevards, storefront streets and greenlink streets

**Policy TC-9** Post public “wayfinding” signs to direct motorists and bicyclists to public destinations within and near Town Center.



**Fig 9.** Wayfinding signs can be located in medians, behind sidewalks, or on poles

**Policy TC-10** Create a seamless network of safe, convenient, and attractive walkway improvements within Town Center that also connects to all streets, the Interurban Trail, high capacity transit on Aurora, and adjacent neighborhoods.

**Policy TC-11** Connect Town Center to other parts of Shoreline and the region by promoting multi-modal transportation choices including high capacity transit on Aurora, frequent local bus service, bicycle paths, and improved pedestrian walkways.

**Policy TC-12** Create safe and attractive pedestrian crossings of Aurora, walkways to better link uses within Town Center, and more direct and attractive walkways from adjacent neighborhoods.

**Policy TC-13** Reduce the noise, visual and safety impacts of traffic on Aurora Avenue as it passes through the Town Center.

**Policy TC- 14** Encourage the removal of the western leg of the intersection at N. 182th and Aurora if re-development of lands at N. 180<sup>th</sup> and Aurora enables the installation of a fully signalized mid-block intersection at that location.

**Policy TC-15** Consider the creation of new rights of way or the vacation of other rights of way in order to facilitate better vehicular and pedestrian circulation. Encourage parcel aggregation and more comprehensive site development designs in order to create a more pedestrian friendly environment and promote mixed use development.

**Policy TC-16** Protect adjacent residential areas from impacts generated by developments in Town Center. Create a medium density buffer between the commercial uses in Town Center and the single family neighborhoods east of Midvale and limit lighting, signage and noise impacts. Orient commercial uses west of Aurora so that they have primary access and impacts oriented toward Aurora, rather than to the neighborhood west of Linden.

**Policy TC-17** Reconfigure Midvale Avenue N. between N. 175<sup>th</sup> St. and N. 182<sup>nd</sup> St. as a low speed, pedestrian-friendly lane with back-in angle parking to support mixed use development on the east side and public uses in the Town Center Park.



Fig. 10 Midvale Ave N. concept with landscaping, crosswalks, and back-in angle parking

**Policy TC-18** Recognize the environmental and aesthetic value of existing stands of prominent trees, promote a green built environment by adopting the U.S. Green Building Code, and launch a recognition program for innovative private projects that exemplify the sustainability vision for Town Center.

**Policy TC-19** Develop the park at Town Center as a memorable, green, open space and link it to the City Hall Civic Center. Program both of these spaces for celebrations, public gatherings and informal “third places.”



Fig. 11 Farmers' markets, parades, lawn sports, and wi-fi access are several possible park uses

**Policy TC-20** Enhance the sustainability of adjacent residential neighborhoods through targeted investments in green street links to Town Center, and focused programs to enhance energy conservation and carbon neutrality.

**Policy TC-21** Encourage structured parking for commercial, multifamily and mixed use developments, and reduce parking requirements in recognition of the availability of transit, on-street parking, walkability, and housing types.

**Policy TC-22** Where feasible, minimize surface parking lots and locate them in rear or side yards and screen them with landscaping, low walls or fences, arbors and other treatments to soften visual impacts.

**Policy TC-23** Celebrate the heritage of the community through preservation, education and interpretation of artifacts and places in or near Town Center. Work with the Shoreline Historical Museum to explore the possibilities for a “Town Center Heritage Walk” and programs to help activate the Park at Town Center.



**Fig. 12** Town Center history: the Red Brick Road, Ronald School House, Interurban Station

**Policy TC-24** Abate the remaining billboards, or re-locate them out of the Town Center, and craft a form-based sign code that orients and sizes commercial signage based on the function and speed of serving streets and walkways.

**Policy TC-25** Create a form-based development code and streamlined permit process that consolidates environmental review and design review into a single expedited administrative permit review. Adopt illustrated and clear design standards with a menu of options and opportunities for design flexibility.

**Policy TC-26** Adopt Town Center design standards and a design review process so that new projects respect existing architectural patterns (e.g., building forms, roof shapes, fenestration, materials, etc.) that provide context and human scale.



**Fig. 13** Town Center roof shapes of various pitches, materials, colors



# **Town Center Zone**

**Draft – 3/31/11**

- 20.92.010 Purpose**
- 20.92.020 Zoning, Land Use, and Form**
- 20.92.030 Street Types and Pedestrian Circulation Map**
- 20.92.040 Neighborhood Protection Standards**
- 20.92.050 Street Frontage Design Standards**
- 20.92.060 Site Design Standards**
- 20.92.070 Building Design Standards**
- 20.92.080 Sign Design Standards**
  
- 20.30.297 Design Review Approval**
- 20.50.021 MUZ Design Review Amendments**
- 20.91.040 Ridgecrest Design Review Amendments**
- 20.20 Definitions**

**20.92.010 Purpose.**

- A. Establish standards for the Town Center Zone. These standards implement the policies of City of Shoreline Comprehensive Plan and Town Center Subarea Plan through code requirements for use, form, design, and process.
- B. Some standards within this chapter apply only to specific types of development and zones as noted. Standards that are not addressed in this chapter will be supplemented by the development standards in Chapter 20.50 SMC. In the event of a conflict between standards, the standards of this chapter shall prevail.
- C. Set forth a procedure designating certain land use actions within the boundaries of the geographic area described in the Town Center Zone Subarea Plan as Town Center Zone as “planned actions” consistent with RCW 43.21.031, WAC 197-11-164 to 197-11-172, and SMC 20.30.640.
- D. Planned action projects that are within the scope of the planned action EIS determination shall not require a SEPA threshold determination and shall be reviewed as ministerial decisions by applying the provisions of the Development Code. Proposed projects that are not within the scope of the planned action EIS shall require environmental review under SEPA
- E. Design Review Approval under SMC 20.30.297 is required for all development proposals prior to approval of any construction permit. A permit applicant wishing to modify any of the standards in this chapter may apply for a design departure under SMC 20.30.297.

**20.92.015 Threshold – Required for site improvements.**

The purpose of this section is to determine how and when the provisions for site improvements cited in the Town Center District development standards apply to development proposals. Full site improvements are required if the development is:

- completely new development; or
- The construction valuation exceeds 50 percent of the existing site and building valuation.

**20.92.020 Zones, Land Use, and Form.**

**A. Town Center Zones**

In order to implement the vision of the Comprehensive Plan's Town Center Subarea Plan, there are Town Center (TC) zones established as shown in Figure 20.92.030.

1. Four zones are delineated within the Town Center that has general and specific design standards.
  - a. TC-1: This zone allows for a broad range of uses similar to TC-2 with the exception to allow vehicle sales, leasing, and servicing.
  - b. TC-2: This zone includes property fronting on Aurora Avenue, N. 175<sup>th</sup> and N. 185<sup>th</sup> streets and provides the widest range of uses and development potential with pedestrian activity primarily internal to the sites.
  - c. TC-3: This zone is oriented toward smaller arterials with a wide range of uses that focus pedestrian activity primarily along street frontages.
  - d. TC-4: This zone is oriented around Stone Avenue and limits the residential heights, uses and vehicle circulation to protect the adjacent single family neighborhoods.
2. Transition Overlay: This overlay provides a transition from higher intensity development to lower intensity uses and protects adjoining single family neighborhoods from large building heights, traffic, and inappropriate land uses.

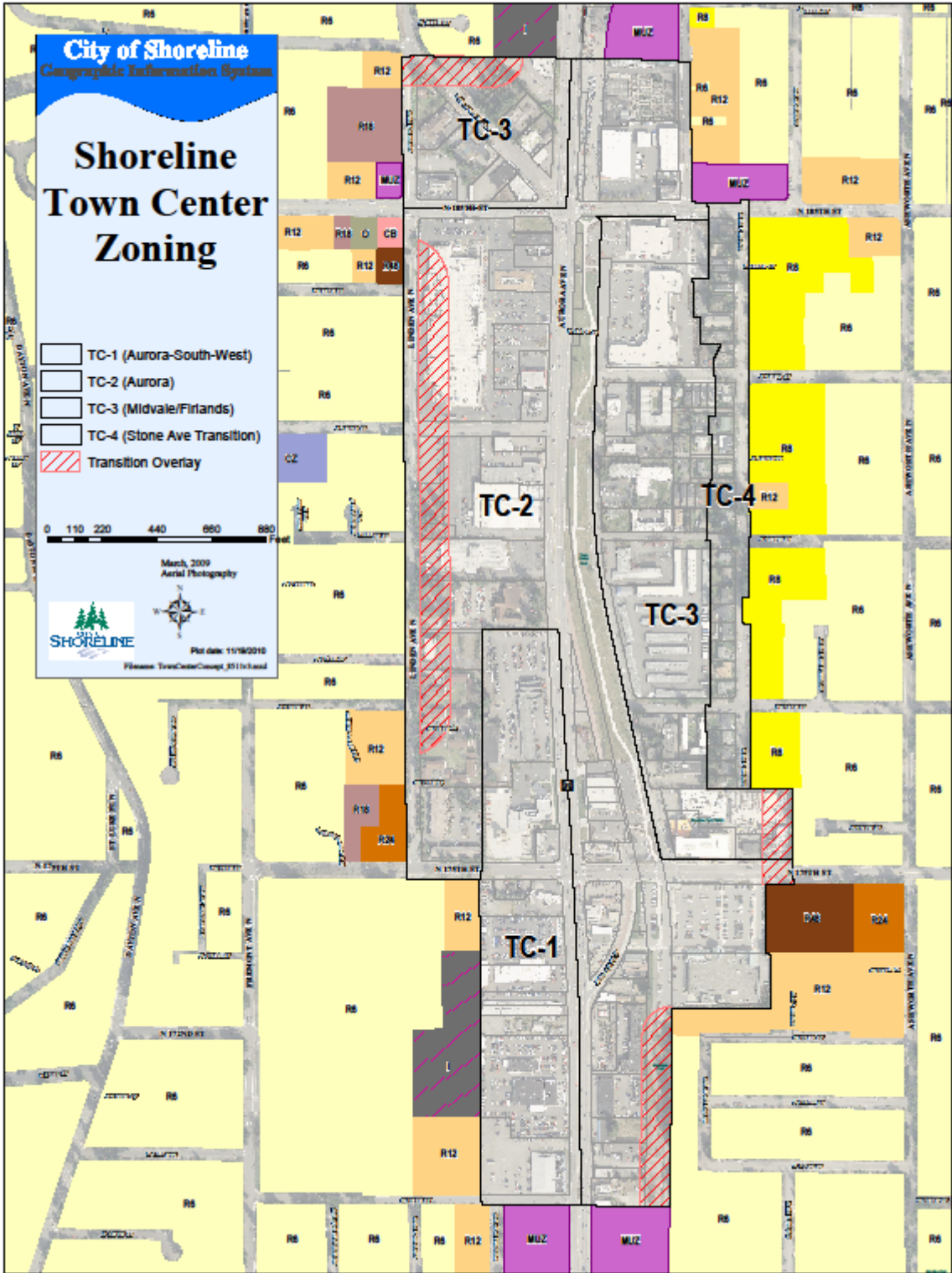


Figure 1. 20.92.020

B. Table 20.92.020(A) lists general categories of permitted land uses for each of the Town Center zones. The general categories for permitted uses include all of the specific uses listed in the corresponding tables cited, except for those listed in this table as “prohibited uses.” If further clarification is required, the Director shall issue an administrative determination consistent with the provisions of this Chapter and the policy guidance of the Town Center Subarea Plan.

**Table 20.92.020(A) Land Use Chart**

General Land Use Category	Specific uses listed in Table	TC-1 Aurora SW	TC-2 Aurora	TC-3 Midvale /Firlands	TC-4 Stone Ave Resid.			
Detached Single Family	20.40.120							
Duplex, Apt, Single Family Attached	20.40.120							
Group Residences	20.40.120							
Lodging	20.40.120	<b>PERMITTED USES</b>						
Health Facility	20.40.140							
Government Facility	20.40.140							
Automotive fueling and service Stations	20.40.130							
Retail, Eating, and Drinking	20.40.130							
Personal and Business Services	20.40.130							
Vehicle Sales, Leasing, and Service(2)	20.40.130							
Gambling Uses								
Wrecking Yards					<b>PROHIBITED USES</b>			
Industrial Uses								
Adult Use Facility								

*Table 20.92.020(A)*

**Table 20.92.020(B) Form.**

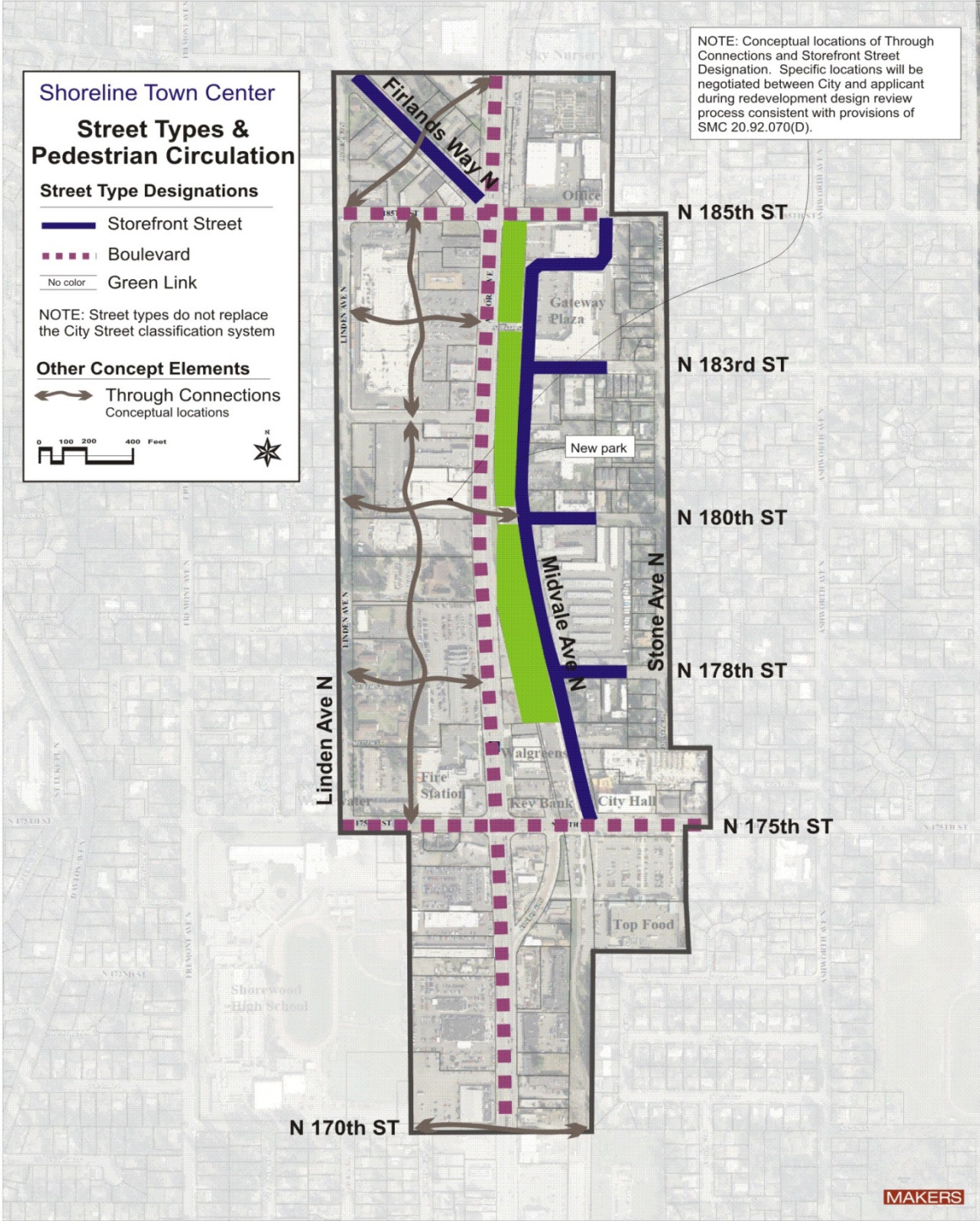
	<b>TC-1 Aurora SW</b>	<b>TC-2 Aurora</b>	<b>TC-3 Midvale/ Firlands</b>	<b>TC-4 Stone Ave Res</b>	<b>Transition Overlay</b>
Minimum Front Yard Setback (1)(2)(3)	0-10 ft (6)	0-10 ft	0-10 ft	15 ft	15 ft
Minimum Side Yard Setback from Nonresidential Zones (4)	0 ft	0 ft	0 ft	5 ft (5)	5 ft (5)
Minimum Rear Yard Setback from Nonresidential Zones	0 ft	0 ft	0 ft	5 ft	0 ft
Minimum Side & Rear Yard (Interior) Setback from R-4 & R-6	15 ft	15 ft	15 ft	5ft	20ft
Minimum Side & Rear Yard Setback from R-8 through R-48 and TC-4	15 ft	15 ft	15 ft	5 ft	15 ft
Maximum Height (5)	70 ft	70 ft	70 ft	35 ft	35 ft
Maximum Hardscape Area	95%	95%	95%	75%	75%

Exceptions to Table 20.92.040(A).

- (1) Unenclosed porches and covered entry features may project into the front yard setback by up to 6 feet. Balconies may project into the front yard setback by up to 2 feet.
- (2) Additional building setbacks may be required to provide right-of-way and utility improvements.
- (3) Front yard setbacks are based on the applicable street designation. See figure 20.92.020 for the street designation and SMC 20.92.070(B) for applicable front yard setback provisions.
- (4) These may be modified to allow zero lot line developments for internal lot lines only.
- (5) See section 20.92.050.C for height step-back standards.
- (6) Front yards may be used for outdoor display of vehicles to be sold or leased in the TC-1 zone.

20.92.030 Street Types and Pedestrian Circulation.

This map illustrates site-specific design elements to be implemented by code for street types and Through Connections.



**20.92.040 Neighborhood Protection Standards.**

A. Purpose

- Minimize negative impacts of Town Center development on adjacent single family neighborhoods.
- Enhance residential neighborhoods on both sides of Linden and Stone Avenue North.

B. Applicability

Unless specifically noted, the standards herein apply to properties within zone TC-4 and the Transition Overlay identified in the Town Center Zoning Map in figure 20.92.030 and other Town Center properties that are directly adjacent to those zones.

C. Building Heights

The maximum building height is 35 feet for the first 50 horizontal feet from the front property line. For each subsequent 20 feet from the property line an additional 10 feet in height is allowed up to the maximum height of the underlying zoning.

D. Site Access

Direct commercial vehicular and service access to a parcel shall not be from Stone or Linden Avenues unless no other access is available or practical as determined by the City.

E. Traffic Impacts

All development in the Town Center shall conduct a traffic impact study and implement traffic mitigation measures which are approved by the city's traffic engineer, to mitigate potential cut-through traffic or parking impacts to single family neighborhoods.

F. Setbacks and Buffers

Buildings in zones TC-2 and TC-3 shall have a 15-foot wide, Type I landscape with an 8-foot solid fence or wall adjacent to zone TC-4 and R-6 parcels in addition to any required open space.

G. Tree Preservation

20 percent of all healthy, significant trees for each parcel must be preserved in TC-4 and Transition Overlays portions of private property per SMC 20.50.290.



**20.92.050 Street Frontage Design Standards.**

**A. Purpose**

- Enhance the appeal of street frontages to encourage people to walk and gather.
- Establish frontage standards for different streets to:
  - Reinforce site and building design standards in each zone.
  - Provide safe and direct pedestrian access within the Town Center and from adjacent neighborhoods.
  - Minimize conflicts between pedestrians, bicyclists, and vehicular traffic and parking.

**B. Applicability**

The standards in this section apply only to the sidewalks and the amenity zone in the public rights-of-way. These standards shall meet the City's Engineering Design Guidelines. Where there is a conflict, the Director shall determine which applies.

**C. Design**

1. Storefront, Greenlink, and Boulevard Street frontages, as depicted on Figure 20.92.030, shall have:

- a. A minimum 10 feet for Storefront streets, 8 feet for Greenlink streets and 7 feet for Boulevard streets of unobstructed sidewalk widths and all streets with 5 feet of amenity zone widths;
- b. Storefront, Boulevard, and Greenlink streets shall have street trees spaced on average 30 feet either in tree pits and grates or in amenity strips;
- c. Storefront and Green Link streets may have breaks in the amenity strip and tree distribution to allow for driveways, site distancing, utilities, crosswalks, bike racks, and benches or sitting walls. In place of amenity strips, street trees in grated pits are required parallel to street parking;
- d. Each development on a Storefront street shall provide a minimum 8 feet of bench or sitting wall;
- e. Both sides of Storefront and Greenlink Streets shall have on-street parking and curb bulb-outs at block ends and pedestrian crossings. On-street parking is optional only if adequate street rights-of-way width do not exist;
- f. Utility appurtenances such as signal boxes, hydrants, poles, or other obstructions shall not be placed in the public sidewalk; and
- g. When improved, Firlands Way within the Town Center shall expose and restore the brick road bed underneath. If restoration of the brick road is unfeasible or cannot meet City road standards then the City shall design a slow street that allows traffic and pedestrians to mix.

2. Rights-of-Way Lighting

- a. One to two-foot candles and maximum 15-foot height for sidewalk areas. Lighting shall be located on private property or mounted on building facades.

**Item 7.a - Attachment 2**

- b. Maximum 25-foot height for street light standards, designed using the Aurora Avenue model and color, modified to meet the 25-foot maximum height, and spaced to meet City illumination standards.



**20.92.060 Site Design Standards.**

A. Purpose

- Promote and enhance public walking and gathering with attractive and connected development to:
  - a. Promote distinctive design features at high visibility street corners.
  - b. Provide safe routes for pedestrians and disabled people across parking lots, to building entries, and between buildings.
- Promote economic development that is consistent with the Town Center Subarea Plan

B. Site Frontage

Site design standards for landscaping, walkways, public places, and open space may be combined if their separate minimum dimensions and functions are not compromised.

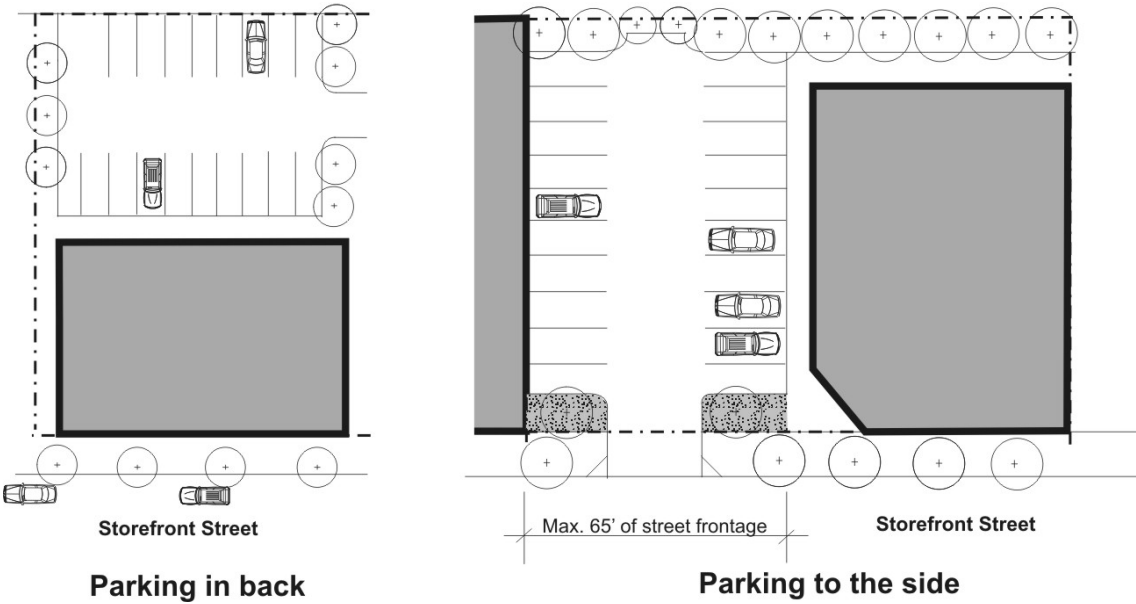
Development abutting street frontages as designated within the Town Center per figure 20.92.030 shall meet the following standards.

1. Storefront Streets

- a. Buildings shall be placed at the property line or back of planned sidewalk if on private property. However, buildings may be setback further if Public Places (as specified in SMC 20.92.070(F)) are included or a utility easement is required between the sidewalk and the building;
- b. Minimum transparent window area is 60 percent of the ground floor facade placed between the heights of 30 inches and 8 feet above the ground for each front facade;
- c. The primary building entry shall be on a street frontage and, if necessary, recessed to prevent door swings over sidewalk or an open entry to an interior plaza or courtyard from which building entries are accessible;
- d. Minimum weather protection at least five feet in depth, along at least 80 percent of the facade width, including building entries; and
- e. Surface parking along Storefront Streets is not more than 65 lineal feet of the site frontage. Parking lots are not allowed at street corners. No parking or vehicle circulation is allowed between the right-of-way and the building front facade. Sites with less than 100 feet lineal feet of frontage are exempt from this standard. See 20.92.070(E)(2) for parking lot landscape standards.



Storefront and Boulevard buildings



Parking lot locations along Storefront streets.

- 2. Green Link Streets
  - a. Minimum front yard setback is 15 feet. Porches and entry covers may project 6 feet into the front yard setbacks;
  - b. Transparent window area is 15 percent of the entire façade;
  - c. Building entries shall be visible and accessible from a street front sidewalk. An entrance may be located on the building side if visible;
  - d. Minimum weather protection is 5-foot deep over building entries;

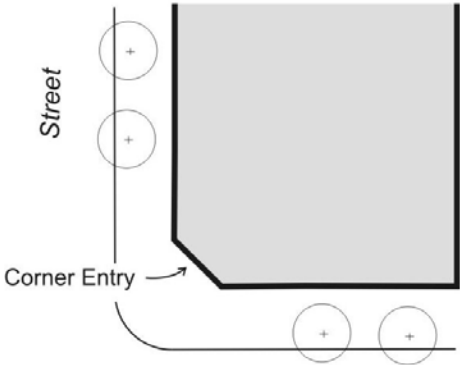
- e. Landscaped front yards may be sloped or terraced with maximum 3 foot high retaining walls; and
  - f. Surface parking is no more than 65 lineal feet of the site frontage and setback 10 feet from property line. Parking lots are not allowed at street corners. No parking or vehicle circulation is allowed between the right-of-way and the building front facade. See 20.92.060(F)(3)for parking lot landscape standards.
3. Boulevard Streets
- a. Developments abutting Boulevard Streets have the option of using Storefront Street or Green Link Street standards or a combination of both standards.
  - b. Surface parking along Boulevard Streets shall not be more than 50 percent of the site frontage. Parking lots are not allowed at street corners. No parking or vehicle circulation are allowed between the right-of-way and the building front facade. Sites with less than 100 lineal feet of frontage are exempt from this standard. See 20.92.070(E)(2)for parking lot landscape standards.



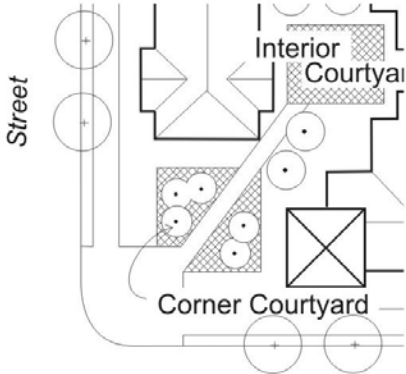
*Landscaped yards*

**C. Street Corners**

- 1. All development proposals located on street corner sites shall include one of the following three design treatments on both sides of the corner.
  - a. Locate a building within 15 feet of the street corner. All such buildings shall comply with building corner standards in paragraph (2) below;
  - b. Provide public places, as set forth in SMC 20.92.070(F) at the corner leading directly to building entries; or
  - c. Landscape 20 feet of depth of Type II landscaping for the length of the required building frontage. Include a structure on the corner that provides weather protection or site entry. The structure may be used for signage (SMC 20.92.100).



Building placed up to the street corner with entry



Public place adjacent to the corner

**Corner Developments**

- 2. Buildings using option 1.a above shall provide at least one of the elements listed below for 40 lineal feet of both sides form the corner:
  - a. 20-foot beveled building corner with entry (included in the 80 lineal feet of corner treatment).
  - b. Distinctive façade (i.e. awnings, materials, offsets) and roofline design.
  - c. Balconies on all floors above the ground floor.
  - d. Minimum 15-foot dimension sculpture or building-mounted artwork
  - e. Other unique treatment as determined by the Director.



**Building corners**

**D. Through-connections and Walkways**

- 1. Developments shall include internal walkways that connect building entries, public places, and parking areas with the adjacent street sidewalks and Interurban Trail. A

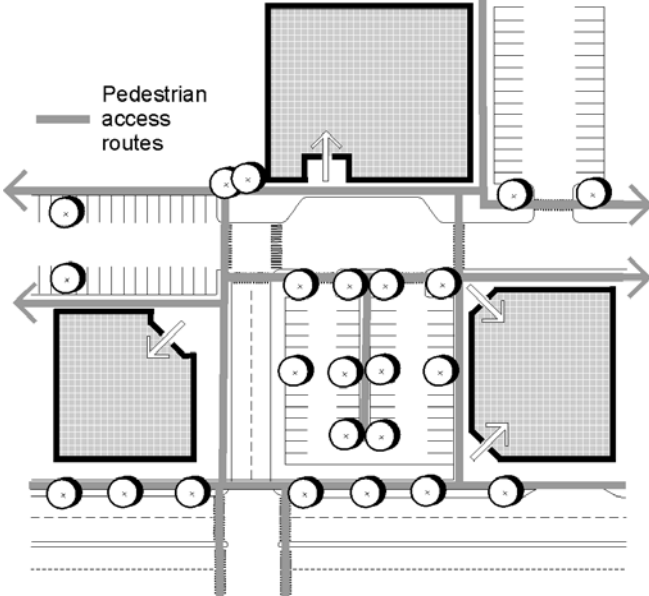
public easement for pedestrian access through properties and city blocks between streets shall be provided for Through-connections, as generally illustrated in the Town Center Concept Plan (SMC 20.92.030).

Walkways and Through-connections shall be connected and may be combined as long as standards of both can be met. The east-west connection aligned with N.180th may be a combination of vehicle access or street and a pedestrian Through-connection. North-south connections can be used as alley access or as a Storefront Street.



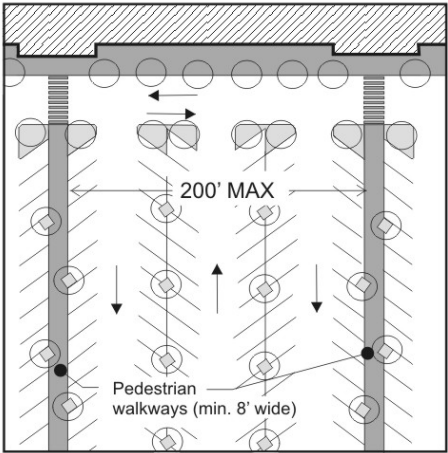
***Through-connections***

- a. All buildings shall have visible, clear, and illuminated walkways between the main building entrance and a public sidewalk. The walkway shall be at least eight feet wide;
- b. Continuous pedestrian walkway shall be provided to the entries of all businesses and the entries of multiple commercial buildings;
- c. For sites abutting underdeveloped land, the Director may require walkways and Through-connections stub-outs at property lines so that future, adjoining development can connect with the pedestrian system;



*Well-connected walkway network*

- d. Raised walkways at least 8 feet in width shall be provided for every three, double-loaded aisle or every 200 feet of parking area. Walkway crossings shall be raised a minimum 3 inches above drives;
- e. Walkways shall conform to the Americans with Disabilities Act (ADA); and



*Parking lot walkway*

- f. Internal walkways along the front facade of buildings 100 feet or more in length must meet Storefront or Boulevard Street standards set forth in SMC 20.92.060(C).





***Internal walkways adjacent to storefronts should be designed to look and function like public sidewalks, including walkway widths and amenity areas.***

- g. Deciduous street-rated trees shall be provided every 30 feet on average in grated tree pits if the walkway is 8 feet wide or in planting beds if greater than 8 feet wide. Pedestrian scaled lighting shall be provided.

**E. Vehicle Parking and Landscaping**

**1. Minimum Off-street Parking**

Parking shall be provided at the following rate:

- a. Residential – .75 space / bedroom.
- b. Retail – 1 space / 400 net square feet.
- c. Civic / Office – 1 space / 500 net square feet.

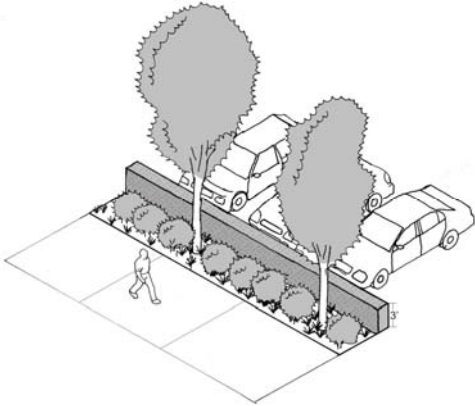
Reductions up to 50 percent may be approved by Director using combinations of the following criteria.

- a. On-street parking along the parcel’s street frontage.
- d. A transit stop within ¼ mile radius.
- e. An off-street public parking lot within ¼ mile radius.
- f. Shared parking agreement with adjoining parcels and land uses that do not have conflicting parking demand.
- g. Commute trip reduction program.
- h. Neighborhood meeting to discuss impacts of traffic and parking.
- i. High-occupancy vehicle (HOV) parking.
- j. Conduit for future electric vehicle charging spaces equivalent to the number of required handicapped parking spaces.

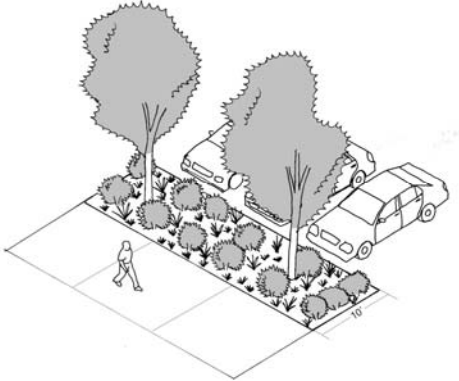
**2. Parking lot landscaping**

The following provisions shall supplement the landscaping standards set forth in Subchapter 7 of SMC 20.50.450. Where there is a conflict, the standards herein shall apply. All parking lots, vehicle display, and loading areas shall meet the following requirements.

- a. Provide a 5-foot wide, Type II landscape that incorporates a continuous masonry wall between 3 and 4 feet in height. The landscape shall be between the public sidewalk or residential units and the wall (see SMC 20.50.460 for details); or
- b. Provide at least 10-foot wide, Type II landscaping.
- c. Vehicle display areas are not required to landscape with trees or shrubs. Transparent security fencing is permitted up to 8 feet. Fencing shall be made of metal other than chain link, razor, barbed, or cyclone material.
- d. Trees shall be placed interior to parking lots at a ratio of one every 10 parking spaces in curbed planters with a minimum dimension of 5 feet.
- e. All parking lots shall be separated from residential development by the required setback and planted with Type I landscaping.



**2a. Parking lot planting buffer with low wall.**



**2b. 10-foot parking lot buffer with Type II landscaping.**

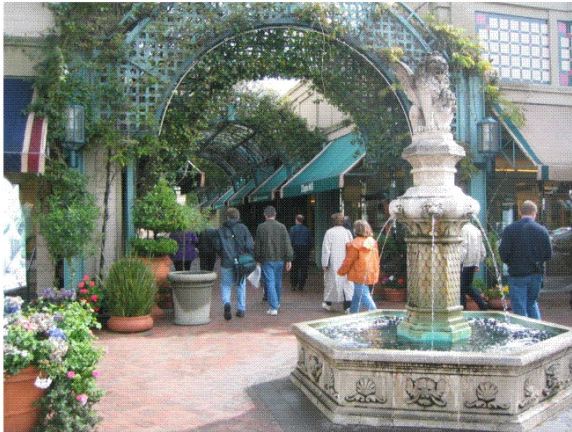
**F. Public Places**

- 1. Public places are required on parcels greater than 1/2 acre with commercial or mixed use development at a rate of 1,000 square feet per acre. Public places may be covered but

**Item 7.a - Attachment 2**

not enclosed. This standard can also be used to meet the standards of walkways as long as the function and minimum dimensions of the public place are met.

- 2. On parcels greater than 5 acres;
  - a. Buildings border at least two sides of the public place;
  - b. The public place shall be at least 5,000 square feet with no dimension less than 40 feet; and
  - c. 80 percent of the area shall be with surfaces for people to stand or sit on.
- 3. On parcels between 1/2 and 5 acres;
  - a. Public places are required to have a minimum dimension of 20 feet; and
  - b. 80 percent of the area shall have surfaces for people to sit or stand on.
- 4. The following design elements are required for public places:
  - a. Physically accessible and visible from the public sidewalks, walkways, or Through-connections;
  - b. Pedestrian access to abutting buildings;
  - c. Pedestrian-scaled lighting (subsection H below);
  - d. Seating and landscaping with solar access at least half of a day, year-round; and
  - e. Not located adjacent to dumpster or loading areas.



*Public Places*

**G. Multifamily Open Space**

- 1. All multifamily development shall provide open space.
  - a. Provide 800 square feet per development or 50 square feet per unit of open space, whichever is greater;
  - b. Other than private balconies or patios, open space shall be accessible to all residents and include a minimum 20-foot dimension including park, playground, roof-top decks or courtyards. This standard can also be used to meet the standards of walkways as long as the function and minimum dimensions of the open space are met;
  - c. Required landscaping can be used for open space if it does not prevent access or reduce the overall landscape standard. Open spaces shall not be placed adjacent to parking lots and service areas without screening; and
  - d. Open space shall provide seating that has solar access at least half of a day, year-round.



*Multi-family open spaces*

**H. Outdoor Lighting**

- 1. All publicly accessible areas on private property shall be illuminated as follows:
  - a. Minimum of one half-foot candles and maximum 25-foot pole height for vehicle areas;
  - b. One to two-foot candles and maximum 15-foot pole height for pedestrian areas;
  - c. Maximum of four-foot candles for building entries with the fixture placed below second floor; and
  - d. All private fixtures shall be full cut-off, dark sky rated and shielded to prevent direct light from entering neighboring property.

I. Service Areas and Mechanical Equipment

- 1. All developments shall provide a designated location for trash and recycling storage and collection. Such elements shall meet the following standards:
  - a. Located to minimize visual, noise, odor, and physical impacts to pedestrians and residents;
  - b. Paved with concrete, screened, and covered in materials or colors that match the building; and
  - c. Located and configured so that the enclosure gate swing does not obstruct pedestrian or vehicle traffic nor require that a hauling truck project into any public right-of-way.



***Trash/recycling closure with consistent use of materials and landscape screening.***

2. Mechanical Equipment

- a. Utility equipment shall be located and designed to minimize their visibility to the public. Preferred locations are off alleys, service drives, within or under buildings or other locations away from the street. Meters and similar equipment shall not intrude into pedestrian areas.



***Utilities consolidated and separated by landscaping elements.***

- b. All rooftop mechanical equipment shall be screened, or colored to be an integral element of the building and minimize visual impacts from the ground level of adjacent streets and properties.

**20.92.070 Building Design Standards.**

**A. Purpose**

- Emphasize quality building articulation, detailing, and durable materials.
- Reduce the apparent scale of buildings and add visual interest.

**B. Façade Articulation**

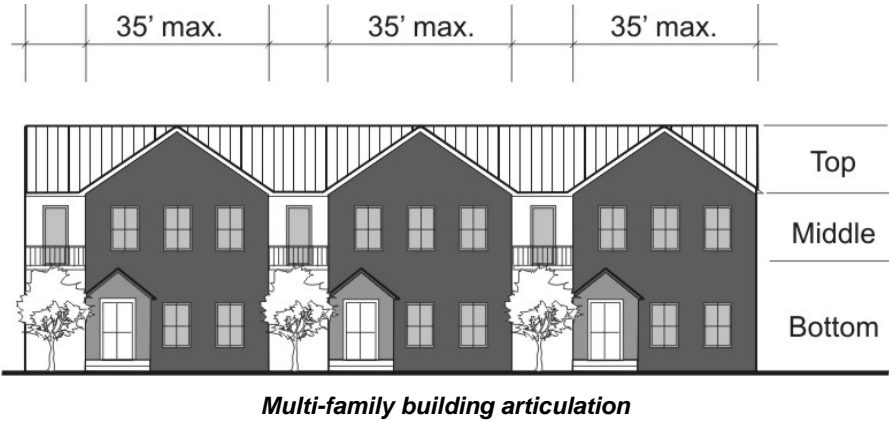
1. All building facing Storefront Streets per Figure 20.92.020 shall include one of the two articulation features set forth in (a) or (b) below no more than every 40 lineal feet facing a street, parking lot, or public place. Building facades less than 60 feet wide are exempt from this standard.



***Storefront articulation***

All buildings facing Boulevard Streets per Figure 20.92.020 shall include one of the two articulation features below no more than every 80 lineal feet facing a street, parking lot, or public place. Building facades less than 100 feet wide are exempt from this standard.

- a. For the height of the building, each façade shall be offset at least 2 feet in depth and 4 feet in width if combined with a change in siding materials. Otherwise, the façade offset shall be at least 10 feet deep and 15 feet wide.
  - b. Vertical piers at the ends of each façade section that project at least 2 inches from the façade and extend from the ground to the roofline.
  - c. Minimum, ground level, interior building space dimension is 12-foot height and 20-foot depth
2. All multifamily buildings or residential portion of a mixed use building facing any street shall provide the following articulation features at least every 35 feet of facade facing a street, park, and public place or open space.
    - a. Vertical building modulation 18 inches deep and 4 feet wide if combined with a change in color or building material. Otherwise, minimum depth of modulation is 10 feet and minimum width for each modulation is 15 feet. Balconies may be used to meet modulation; and
    - b. Distinctive ground or first floor façade, consistent articulation of middle floors, and a distinctive roofline or articulate on 35 foot intervals.



***Multi-family building articulation***

**3. Roofline Modulation**

Rooflines shall be modulated atleast every 120 feet by emphasizing dormers, chimneys, stepped roofs, gables, or prominent cornices or walls. Rooftop appurtenances are included as modulation. Modulation shall consist of a roofline elevation change of at least four feet every 50 feet of roofline.

**4. Maximum Façade**

A building exceeding 150 feet in length along the street front shall have a minimum 30-foot wide section that is offset at least by 20 feet through all floors.



***Façade widths using a combination of façade modulation, articulation, and window design.***



5. Windows

Buildings shall recess or project individual windows above the ground floor at least two inches from the façade or incorporate window trim at least four inches in width or a color that contrasts with the façade color.



***Window trim design***

6. Secondary Entry

Weather protection at least 3 feet deep and 4 feet wide is required over each secondary entry;



***Covered secondary public access***

7. Façade Materials

- a. Metal siding shall have visible corner moldings and trim and shall not extend lower than six feet above grade. Masonry, concrete, or other durable material shall be incorporated between the siding and the grade. Metal siding shall be factory finished, with a matte, non-reflective surface.



***Masonry or concrete near the ground and proper trimming around windows and corners.***

- b. A singular style, texture, or color of concrete block shall not comprise more than 50 percent of a façade facing a street or public space.



***The left image uses smooth gray blocks on the vertical columns and beige split-faced blocks above the awnings. The storefront in the right image uses gray split face and some lighter, square, smooth-faced blocks below the storefront windows.***

- c. Synthetic stucco must be trimmed and sheltered from weather by roof overhangs or other methods and are limited to no more than 50 percent of façades containing an entry and shall not extend below 2 feet above the grade.



*Concrete near the ground level and a variety of other surface materials on the façade.*

8. Prohibited materials.
  - a. Mirrored glass, where used for more than 10 percent of the façade area.
  - b. Chain-link fencing.
  - c. Fiberglass sheet products.
  - d. Plywood siding.

**20.92.080 Sign Design Standards.**

**A. Purpose**

- Require signage that is both clear and of appropriate scale for the project.
- Enhance the visual qualities of signage through the use of complementary sizes, materials and methods of illumination.
- Require signage that contributes to the character of Shoreline's Town Center.

**B. Applicability**

The sign standards herein shall supplement the provisions of SMC 20.50.540. Where there is a conflict, the provisions herein shall apply.

**C. Permitted Illumination**

1. Channel lettering or individual back-lit letters mounted on a wall or individual letters placed on a raceway, where only light shines through the letters.
2. Opaque cabinet signs where light only shines through letter openings.
3. Shadow lighting, where letters are backlit, but light only shines through the edges of the letters.
4. Neon signs
5. Externally lit signs



*Individual backlit letters (left image), opaque signs where only the light shines through the letters (center image), and neon signs (right image).*

**D. Monument Signs**

1. One sign is permitted per frontage, per property, and regardless of the number of tenants. An additional monument signs is permitted on a property if the frontage length is greater than 250 feet and the signs are at least 150 feet apart.
2. Use materials and architectural design elements that are consistent with the architecture of the buildings.
3. Signs in Zone TC-3: Maximum height: 6 feet and maximum area: 50 square feet per sign face.
4. Signs in zones TC-1 and TC-2 when placed along Aurora Avenue, N. 175<sup>th</sup> or N. 185<sup>th</sup> streets. Maximum height: 12 feet and maximum area: 100 square feet per sign.

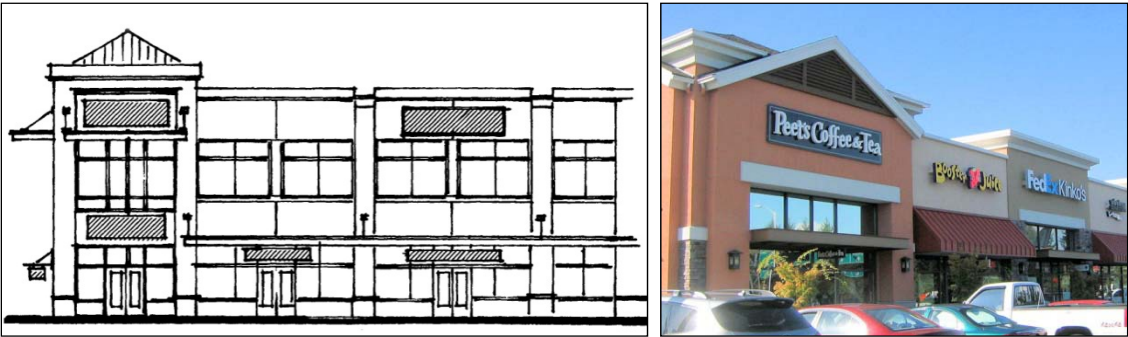
- 5. Signs may be placed up to the front property line if site distancing and public safety standards are met.
- 6. Signs shall be setback from the side property lines at least 20 feet.



*Monument sign*

**E. Building Signs**

- 1. Each tenant or commercial establishment is allowed one building sign - wall, projecting, marquee, awning, or banner sign per facade that face the adjacent streets or customer parking lot.
- 2. Building signs shall not cover windows, building trim, edges, or ornamentation.
- 3. Building signs may not extend above the parapet, soffit, the eave line, or on the roof of the building.
- 4. Each sign area shall not exceed 25 square feet for Zone TC -3 and 50 square feet for zones TC-1 and TC-2.
- 5. The sign frame shall be concealed or integrated into the building's form, color, and material.



*Signs are centered on architectural features of the building.*

- 6. Projecting, banner, and marquee signs (above awnings) shall clear sidewalk by 9 feet and not project beyond the awning extension or 8 feet, whichever is less. These signs may project into public rights-of-way for storefront buildings, subject to City approval.



***Projecting sign***

**F. Under-awning Signs**

- 1. Not extend within 1-foot of the awning outer edge and the building façade;
- 2. Minimum clearance of 9 feet between the walkway and the bottom of the sign;
- 3. Not exceed 2 feet in height; and
- 4. One sign per business.

**G. Windows signs are exempt from permits but cannot exceed 25 percent of the window area**

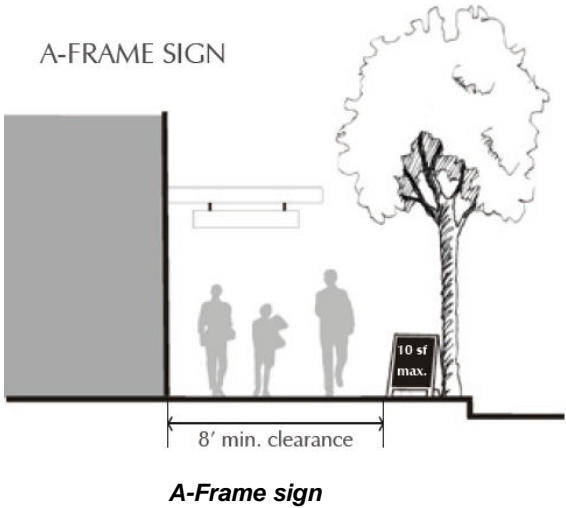


***Under-awning signs***

**H. A-Frame or Standing Signs**

- 1. One sign per business;
- 2. Must be directly in front of the business;
- 3. Cannot be located within the 8 foot sidewalk clearance on designated Storefront Street and 5 feet on all other sidewalks and internal walkways;
- 4. Shall not be placed in landscaping, within 2 feet of the street curb where there is on-street parking, public walkways, or crosswalk ramps.
- 5. Shall not exceed 6 square feet per side; and

6. No lighting of signs is permitted.



I. Transition Overlay and Zone TC-4 Signs

All signs in the Transition Overlay and Zone TC-4 shall meet residential sign standards of SMC 20.50.540(B).

J Prohibited signs

- 1. Pole signs.
- 2. Billboards.
- 3. Electronic changing message or flashing signs.
- 4. Backlit awnings used as signs.
- 5. Other signs set forth in SMC 20.50.550.

**Table 20.30.040** – Summary of Type A Actions and Target Time Limits for Decision, and Appeal Authority

Action Type	Target Time Limits for Decision	Section
<b>Type A:</b>		
1. Accessory Dwelling Unit	30 days	20.40.120, 20.40.210
2. Lot Line Adjustment including Lot Merger	30 days	20.30.400
3. Building Permit	120 days	All applicable standards
4. Final Short Plat	30 days	20.30.450
5. Home Occupation, Bed and Breakfast, Boarding House	120 days	20.40.120, 20.40.250, 20.40.260, 20.40.400
6. Interpretation of Development Code	15 days	20.10.050, 20.10.060, 20.30.020
7. Right-of-Way Use	30 days	12.15.010 – 12.15.180
8. Shoreline Exemption Permit	15 days	Shoreline Master Program
9. Sign Permit	30 days	20.50.530 – 20.50.610
10. Site Development Permit	60 days	20.20.046, 20.30.315, 20.30.430
11. Deviation from Engineering Standards	30 days	20.30.290
12. Temporary Use Permit	15 days	20.40.100, 20.40.540
13. Clearing and Grading Permit	60 days	20.50.290 – 20.50.370
14. Planned Action Determination	28 days	20.90.025
15. Design Review	28 days	20.30.297

An administrative appeal authority is not provided for Type A actions, except that any Type A action which is not categorically exempt from environmental review under Chapter 43.21C RCW or for which environmental review has not been completed in connection with other project permits shall be appealable. Appeal of these actions together with any appeal of the SEPA threshold determination is set forth in Table 20.30.050(4). (Ord. 531 § 1 (Exh. 1), 2009; Ord. 469 § 1, 2007; Ord. 352 § 1, 2004; Ord. 339 § 2, 2003; Ord. 324 § 1, 2003; Ord. 299 § 1, 2002; Ord. 244 § 3, 2000; Ord. 238 Ch. III § 3(a), 2000).



**20.30.297 Design Review (Type A)**

Design Review approval shall be granted by the Director upon his/her finding that:

1. The design meets the requirements of the applicable code subsections.
2. The design improves the function, continuity, connection, or pedestrian interest from building to building or site to site.
3. The choice of materials and architectural elements is compatible with the context of other development in the vicinity.
4. Departures from the design standards in the applicable chapter shall be consistent with the purposes or intent of each subsection or be justified due to unusual site constraints so that meeting the design standards represents a hardship to achieving full development potential.
  - a. For the Town Center District, dimensional standards in Table 20.92.030 regarding setbacks and building envelope cannot be modified by Design Review.

**20.50.021 Development in the mixed-use zone (MUZ)**

Development in the MUZ zone shall meet the following requirement:

- A. All developments in the MUZ zone ~~are subject to administrative design review as approved by the Director. The Director is authorized to adopt and amend design guidelines by administrative order~~ are subject to Design Review Approval in SMC 20.30.297.

**20.91.040 Administrative Design review. (Ridgecrest Planned Area)**

- A. **Applicability.** ~~Administrative~~ Design review will be required for developments in Ridgecrest Commercial Planned Area 2 that are 1.5 acres or more and that meet one of the thresholds in SMC 20.50.125.
- B. **Standards for Approval.** When design review is required, the applicant will demonstrate that plans satisfy the criteria in SMC 20.30.297. ~~20.91.050 unless approved as a design departure by the department director consistent with the intent of each subsection.~~
- C. **Design Modifications. Departures.** A permit applicant wishing to modify any of the standards in this chapter may apply for a design departure under SMC 20.30.297. ~~A design departure will be approved if it is consistent with the intent of each subsection and it meets or exceeds the standard design objective. The director's decision may be appealed to the hearing examiner with substantial weight given to the director's decision.~~

**20.20 Definitions.**

The following definitions apply to Chapter 20.

Building articulation	The emphasis to architectural elements (like windows, balconies, entries, etc.) that create a complementary pattern or rhythm, dividing large buildings into smaller identifiable pieces. See SMC 20.92.180 for applicable standards.
Banner sign	A sign constructed of cloth, canvas, or other similar light weight material that can easily be folded or rolled, but does not include paper or cardboard.
Boulevard Street	Refers to a street and/or segment of a street where there's an option for commercial storefronts or landscaped setbacks along the street with the option of ground floor residential or commercial uses.
Frontages	Facilities between the curb and private development along streets – typically curbs, amenities, and sidewalks.
Green Link Street	Refers to a street and/or segment of a street envisioned to have or maintain landscaped building setbacks along the street. See Figure 20.92.030 for the location of designated Landscaped Streets and SMC 20.92.070(B)(3) for the description and applicable standards for properties fronting on designated Landscaped Streets.
Modulation	A stepping back or projecting forward of portions of a building face, within specified intervals of building width and depth, as a means of breaking up the apparent bulk of a structure's continuous exterior walls.
Public places	See SMC 20.92.140 for the description, standards, and guidelines for public places.
Roofline Modulation	Refers to a variation in roof form. See SMC 20.92.180 for provisions.
Storefront	A pedestrian-oriented façade placed up to the edge of a public sidewalk. See SMC 20.92.070(C)(1).
Storefront Street	Refers to a street or segment of a street where envisioned to have storefronts placed up to the edge of the sidewalk. See figure 20.92.030 for the location of designated Storefront Streets and SMC 20.92.070(B)(1) for the description and applicable standards for properties fronting on designated Storefront Streets.

Transparent window	A window that is capable of transmitting light so that objects or images can be seen as if there were no intervening material variation in roof form.
Trellis	A frame supporting open latticework used as a screen or a support for growing vines or plants.
Walkways	On-site hard surfaces for pedestrian and non-motorized circulation.